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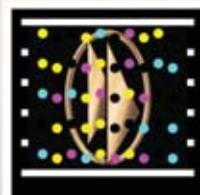
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On The Cover: We went on the road with — and in — the Fiat 500, a cute two-door hatchback that's more than suitable for dinghy towing (see test on page 36). Photo by Chris Hemer.

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ECONOMIES OF SCALE



As the final final days of spring make way for the transition into the summer travel season, it's time to bust out the campground directories, peruse the Internet and, in many cases, consult with motorhoming friends to make plans for our next trips. Once again our plans may be impacted by financial considerations as fuel prices head in the wrong direction. At press time, it seemed like the busiest people in this country were fuel station workers who physically change the numbers on the price signs. Add the hysteria perpetuated by the media, and many motorhome owners will likely alter travel plans for this summer.

Granted, fuel costs are going to be higher this summer and the cost of vacationing will increase, but as a motorhoming friend said the other day as we commiserated over fuel costs, "I'm not going to cancel my trip this summer for a couple hundred bucks." With respect for owners with financial concerns, I agree wholeheartedly.

The good news is that if fuel prices are indeed rising in response to consumer demand and other related factors, the economy is probably heating up a little. Although no one wants to pay more for fuel, I'll be glad to swap the extra cost to power our rigs — within reason, of course — for the return to happier economic times.

There are ways to ease the additional financial burden without giving up things we love about the motorhome lifestyle. We might be less tempted to just point our motorhomes in a general direction and explore what's around the next bend. Moving around certainly satisfies our need to wander, but rolling up the miles can be expensive. Pinpointing a destination and staying in an RV park for longer periods of time is much more cost effective. From there, it's easy to take in the local sights and attractions using our dinghy vehicles.

Fortunately, many of the vehicles we're towing behind our motorhomes are fuel-efficient. And most of these cars and SUVs are also enjoyable to drive. I hated to give

back the Fiat 500 we tested for this month's cover story. As author Chris Hemer spells out in his evaluation on page 36, the diminutive Fiat is not just small, it's a car that's really fun to drive. And I can second that. Combine spirited performance and easy maneuverability with terrific gas mileage, and the Fiat is surely a winner for use in dinghy service — and as a daily commuter.

But it's not always about parking our motorhome in a campground and spending the days away from our sites. The trend is moving toward full-service RV parks that have plenty to offer their temporary residents. It's the formula Europeans have been using for many years and we're seeing many more RV parks in the United States transforming their facilities into resorts packed with activities and features.

Attractions are usually custom tailored to the demographics of the parks' guests. Planned activities in the Sunbelt, where snowbirds hang out for months, keep residents occupied and fit, often employing professional activities directors. Parks designed for families have pools and game rooms to keep the youngsters busy. We visit a park every year during the summer that offers movies projected on a huge screen adjacent to the pool — "really cool" has a double meaning here.

I predict that RV parks of the future will adopt many of the all-inclusive features found in popular hotel resort destinations. Don't feel like cooking? Visit the restaurant on premises. Looking for a glass of wine? Hang out at the club near space 235. Want to be entertained? Stroll over to the amphitheater for a concert under the stars.

If resort camping is not your thing, it's just as easy to stay put in a beautiful national or state park and partake in outdoor activities. In either case, the economies of scale will work in our favor. ♦

MOTORHOME

PUBLISHER: Bob Livingston
 EDITOR-IN-CHIEF: Eileen Hubbard
 MANAGING EDITOR: Laura Michaels
 ASSOCIATE EDITOR: Amanda Lepinski
 ART DIRECTOR: Randy Kepner
 PRODUCTION MANAGER: Angela Schmiegl
 SALES ADMINISTRATOR: Erica Thiel

CONSULTING EDITORS: Wes Caughlan, Ken Freund, Jeff Johnston
 CONTRIBUTORS: Chuck Campbell, Bill and Jenn Gehr, Bobbie Hasselbring, Chris Hemer, Fred Pausch, E. Don Smith, Bill Stoughton, Gary and Monika Wescott, Mary Zalmanek

VICE PRESIDENT NATIONAL SALES: Terry Thompson
 RV MARKETPLACE: Sue Seidltz
 PO Box 8510, Ventura, CA 93002
 Tel 805-667-4100 Fax 805-667-4379

Tacy Hendershot-Sargent (Southeast Region)
 1010 SW Lighthouse Drive, Palm City, FL 34990
 Tel 772-288-0387 Fax 772-288-0085

Lou Cicirelli (Northeast Region)
 1400 NW 108th Avenue, #266, Plantation, FL 33322
 Tel 954-297-9234

Scott Oakes, John Marciano (Seattle, WA)
 1818 Westlake Avenue, N. #420
 Seattle, WA 98109
 Tel 206-283-9545 Fax 206-283-9571

Crompton Holdings Scott Crompton (Detroit, MI)
 2031 Kings Highway, Shreveport, LA 71103
 Tel 334-546-7243 Fax 334-356-7740
 scottcrompton@mac.com

CHAIRMAN EMERITUS: Art Rouse
 PUBLISHER EMERITUS: Bill Estes

Production Specialist Cherri Perschmann, Digital Advertising and Marketing Manager: Jeffrey Larson, Senior Marketing Manager: Kathryn Knudson, Administrative Assistant-Audience Development: Molly Sullivan, Digital Media Content Editor: Mike Davin, Digital Media Manager: Chris Soash, Digital Marketing Specialist: Kathryn Benson, Digital Editor: Nick Upton, Associate Editor: Chris Gerber



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 SENIOR VICE PRESIDENT/PRODUCTION: Barb Hammer
 VICE PRESIDENT/ADVERTISING SALES: Terry Thompson
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HOW CLOSELY DO YOU ADHERE – ABOVE OR BELOW – TO THE POSTED SPEED LIMITS ON FREEWAYS VERSUS SMALLER TWO-LANE HIGHWAYS? AND, SHOULD SPECIAL RV/TRUCK SPEED LIMIT SIGNS BE MORE WIDESPREAD?

That's the question we asked in our February issue, and here are some of the replies we received.

MIND YOUR MPH

My husband usually drives 2 or 3 MPH under the limit on freeways, but when the GPS shows red I remind him to slow down. We try not to speed because we want to be safe on the road; however, sometimes you need to go with the flow of traffic.

ELMER AND SYLVIA DETURCK | VIA EMAIL

TIPS ON SAVING

We are hoping that a lot of little changes will result in recognizable savings while on the road. My dinghy vehicle has synthetic lubricants to reduce friction and we are back to driving 55 MPH. We find it less stressful and it does save fuel. When it's time to look for a park for the night or longer, we look for city, county or state campgrounds. Most city or county campgrounds offer significant savings while providing the needed amenities. For a stay in a commercial RV park, electric, water and a dump station will lower the rates. When we shop for groceries we look for a sale flyer at the store entrance. We have decided to limit eating out. The savings are significant and it's a healthier alternative. I told my wife we will have to fish along the way to cut food cost. I'm hoping she will buy it.

RAY JODOIN | NORTH PORT, FLA.

HAPPY WITH HAPPY BOWL

In the February issue there was an article on the Happy Bowl liners ("Hands-On"). We have been camping part time since 2009 but I have been reading various RV magazines since 2008. I had never seen anything on this subject and was just about to make my own version when we went to the New Hampshire Samboree in 2011 and they had samples there. I was so happy to see them and have since ordered the 50-pack. They work as shown and we are again happy campers.

DAVE AND CAROL ROGERS | AUBURN, N.H.

NOT MADE TO MELT

I read with interest the article on LED lighting ("The Light-Emitting Diode Revolution," February). One side effect not pointed out is that LED taillights are not as likely to melt

snow from their surface because of the lower operating temperatures, with the potential of obstructed visibility from the rear in adverse winter weather.

JIM GOCHNAUER | VIA EMAIL

LAZYDAYS ANYTHING BUT LAZY

Last November we took our motorhome to a local repair shop to chase down a small electrical issue. While there our coach suffered substantial damage to the full-wall slide. The repair required body and mechanical work, with the probability of having to pull the slide.

We live in a small town in northeastern Arizona and called several shops in the greater Phoenix area. We were told that either they didn't have the capability of pulling the slide or that they would not guarantee the work.

Thank goodness I remembered that Lazydays had just opened a shop in Tucson. My husband called and talked to Dale Frei. Dale assured us Lazydays could provide the complete repair from beginning to end and guarantee the work for as long as we owned the coach.

We dropped off the coach at the Tucson facility and met Dale. He walked us through the secure enclosed shop where our coach would be kept during the length of the repair. We cannot say enough about how good our experience was at Lazydays. Not only did Dale keep in contact with us throughout the repair process, he also sent us pictures along the way.

So, if you ever find yourself in Tucson and in need of service for your rig, we recommend the folks at Lazydays.

LEE ANNE HAGEDORN | SHOW LOW, ARIZ.

MONEY-SAVVY MOVIE OPTION

While I greatly enjoy the "Hands-On" column and have several of Bob Livingston's books, a few months ago it missed the mark.

I was recently flipping through the June 2011 issue, and "Hands On" profiled a mobile movie server for RVers. I nearly fell over when I read the price: \$1,495. There is no way I would spend that kind of money on a DVD player. I have a unit here at my house (that moves to the RV on trips) that does exactly

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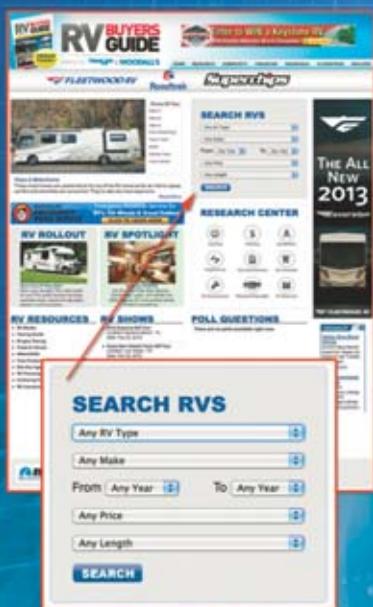
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the same thing. And not only is it about half the size, it holds more movies than the unit reviewed and is only \$200. I went to Fusion Research's website and saw that its Rocket Mobile Server is now \$1,795 and the DVD drive is an extra \$95 on top of that. The article made no mention of an additional cost for the DVD drive.

The only people who would consider spending nearly \$2,000 on a DVD player and built-in storage device are those who are driving coaches in the top 5 percent of the price range. Or, as our buddy who is in that top 5 percent puts it, "The More Money Than Brains Club."

The unit I have is a Western Digital WD TV Live Hub. I bought it at Best Buy for \$200 and it comes with 1 terabyte of storage and a remote as well. You connect this unit to the TV through an HDMI cable and access the movies from your TV's remote (not a separate remote). You do need a computer with a DVD drive to load movies, but almost all RVers have this already so an external DVD drive is not needed. It was a snap to set up, is easy to play movies and didn't make my wallet too light!

I just wanted to offer the motorhome community another more affordable option. Thanks for all you do and safe travels.
NEIL HARSCH | SAN ANTONIO, TEXAS

PLAY IT BY EAR

In answer to the January "P.O. Box" question regarding booking overnight sites in advance, our answer is no. We usually play it by ear, as our vacations tend to be very relaxed with a general itinerary in mind. For example, we have decided to head up the Oregon coast and that may or may not include going up into Washington or even British Columbia. It is a rarity that we have a problem and then it just means we wander up the road a little farther before stopping. This way, there is no pressure to be somewhere that we thought we might be by a certain day. It gives us the freedom to linger or wander off as we please. We hate having to make reservations!

ED HIBBS | CLOVIS, CALIF.

MUCH ADO ABOUT NOTHING

I would like to sue Winnebago for building an entirely perfect, flawless RV — the Winnebago View Profile — thus denying me my birthright to complain, whine and

moan. I need to be a victim and have people feel sorry for me. I demand pity, darn it. Instead, I am forced to enjoy endless days of bliss on the highway, camping, enjoying munchies with my wife, nice hot showers and air-conditioned nights in a comfortable bed. And the Sprinter chassis is quiet, comfortable, fun to drive and economical on fuel. Is there no end to my nightmare? Is there no justice?

VERNON FINCH
IQUALUIT, NUNAVUT, CANADA

LAKE MISTAKE

The picture on page 15 of the February issue is incorrect (Escapes, "Diamond In the Wilderness"). The article highlights Alberta's Lake Louise as a vacation gem. The only problem is that the picture is not of Lake Louise. The lake pictured is Moraine Lake, which is located south of Lake Louise. Otherwise, a great article and magazine.

TOM SCHROER | VIA EMAIL

Editor's note: We were directed to photo options through a press representative with the Banff Lake Louise/Banff National Park tourism website. The one chosen was of Moraine Lake rather than Lake Louise. We apologize for any confusion this has caused the readers of MotorHome and thank those who contacted us regarding the photo.

WANTED: SITE SUGGESTIONS

I am the new guy on the block when it comes to motorhomes. We had a travel trailer for the past 12 years and we just bought a Class A motorhome. I would like to plan a trip south in spring that will take us seven or eight hours driving time from our home in Hudson, Mass. Does anyone have suggestions for the new motorhome owners on the road?

AL AND FRAN TEMPLE
HUDSON, MASS.

QUESTION:

What are the must-see sites you've encountered heading south along the East Coast and what makes them so special?

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ASK A TECHNICAL QUESTION: Questions for our Coach & Powertrain column may be sent to MotorHome, 3300 Fernbrook Lane N #200, Minneapolis, MN 55447 (please include your name, city and state). You may also email questions to tech@motorhomemagazine.com. Selected letters will be answered in the magazine, but time does not permit individual replies.

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NECESSARY STUFF

“Wow! Look at all the storage in that motorhome’s exterior cabinets.”

That’s what you think when you see a brand new coach sitting there with the storage compartments empty. And then you start filling them up with all the “necessary” stuff you have to carry.

Here’s an inventory of what we have determined is necessary stuff in the outside compartments of our 36-foot motorhome. We didn’t start out with all this stuff. It just gradually grew to meet demand.

The space adjacent to the hookup compartment contains the 30-foot-long, 50-amp shorepower cord that came with our motorhome. For those occasions when 30 feet is not long enough, I have added a 25-foot-long, 30-amp power cord and an adapter to connect the two cords together.

Actually, we have two 30-amp cords. So it’s possible for me to reach out 80 feet and tap into an electrical outlet. We also have an adapter that allows me to attach the 30-amp cord directly to our motorhome when only a 30-amp hookup is available. Another adapter allows me to connect a 30-amp cord to a 15-amp outlet. Yet another adapter makes it possible to plug a 30-amp cord into a 50-amp outlet. There is also a 50-foot-long extension cord. Now, I wouldn’t try to

connect all those cords together and plug into a 15- or 20-amp outlet, but having an assortment of cords and adapters allows me to cover a variety of situations.

In the same compartment we also have four lengths of sewer hose — one 8-footer and three 10-footers. Each has a male connector at one end and a female connector at the other. Another 2-foot length of sewer hose has a female connector at one end and a 90-degree sewer adapter at the other. Most of my sewer hookups are usually within 12 feet of my sewer outlet but I can easily connect all the hoses and reach out as far as 35 feet. If it’s any farther away, I’ll just have to do without.

A 50-foot length of green garden hose is also in this compartment. I have used this on more than one occasion to empty our gray-water holding tank into a non-conventional sewer inlet (a sewer cleanout access, for example). The hose

is green so I don’t accidentally use it for fresh water.

Depending upon the distance to the water hookup, I have my choice of either a 10-, 15- or 25-foot length of water hose (or 50 feet if they are joined together). My water filter is next. And finally, 4 feet of water hose that connects the water filter to the motorhome’s water inlet. The water filter and hoses are equipped with quick-connect fittings. Connecting one to the other is literally a snap. By the way, the fresh-water equipment never comes in contact with the sewer hookup equipment.

Two 15-foot lengths of coaxial antenna cable have proven adequate so far. So has a 20-foot length of telephone extension cord for those rare occasions when a telephone hookup has been available and useful.

A small box contains a circuit analyzer, a box of disposable gloves, a pair of pliers for tightening and loosening hose fittings, a hose-repair kit, water-hose washers, a variety of TV cable connectors, and, of course, a roll of duct tape.

Vicki also converted one exterior compartment into an enormous pantry.

The remaining storage compartments contain a short stepladder, two folding chairs, an assortment of leveling boards and a toolbox. There is also a doormat we place on the ground outside our entry door. It allows the dirt to fall through to the ground when I remember to wipe my feet. Plus, there’s a 3-by-5-foot outdoor mat; we lay this outside our door when the ground is particularly dusty or sandy. It also does duty as a mat to lie on when working on the underside of our motorhome. And, finally, a 50-foot length of air hose lets me tap into our diesel’s air-compressor and add air to the tires. The hose has not been used since I acquired a more convenient 125 PSI 120-volt AC air compressor.

There’s an RVing rule somewhere that states, “the amount of stuff you carry is directly proportionate to the amount of storage space available.” I guess if we had more space we would carry more stuff.

Visit the Kievas’ website at www.rvknowhow.com. ♦

THERE’S AN RVING RULE SOMEWHERE THAT STATES, ‘THE AMOUNT OF STUFF YOU CARRY IS DIRECTLY PROPORTIONATE TO THE AMOUNT OF STORAGE SPACE AVAILABLE.’ I GUESS IF WE HAD MORE SPACE WE WOULD CARRY MORE STUFF.



escapes

by AMANDA LEPINSKI

EVENTS | NOTEWORTHY | CROSSROADS | WHEELS & GEAR

PHOTO: CHRISTIAN DEJEN

FLORA AND FAUNA ON THE HORIZON

DRIVING ACROSS SOUTH DAKOTA'S WILDLIFE LOOP SCENIC BYWAY

The Wildlife Loop Scenic Byway, located within Custer State Park, stretches for 18 miles and has something to offer every traveler. Nestled among South Dakota's famous Badlands, the drive guarantees gorgeous views of prairies and mountain foothills. Named accordingly, this byway also gives travelers plenty of exciting photo ops filled with bison, bighorn sheep, elk, coyotes, prairie dogs and burros. The Wildlife Loop Scenic

Byway also offers beautiful views any time of the day. A little fact: the world's largest publicly owned bison herd lives at Custer State Park. The park also offers 13 campgrounds, including a horse camp and French Creek Natural Area, for a more primitive camping experience.

For more information about the byway and Custer State Park, visit www.southdakota.com or call 605-255-4515.

MAY 5-12 | Since 1929, the city of Holland, Mich., has celebrated **Tulip Time**, a Dutch festival honoring the tulips blossoming across the city. The festival has grown throughout the years and now includes trolley tours of the city, an art and craft fair, klompen (wooden clogs) dance, quilt show, fireworks and Dutch marktplaats — an area dedicated to homemade Dutch food, folk dancing and crafts; 800-822-2770, www.tuliptime.com.

MAY 12-13 | More than 10,000 people come out each year to the **Prescott Fine Art & Wine Festival** to view more than 140 juried artists showcasing and selling their work, including paintings, sculptures, ceramics, mixed media, photography and jewelry. The art is displayed among the elm trees at the Historic Courthouse Plaza in downtown Prescott, Ariz. Attendees taking a break from art-gazing can enjoy a glass of Arizona's finest wine at the wine garden, where delicious food from Prescott-area restaurants is also available; 928-445-2510, www.prescottartfestivals.com.

MAY 18-20 | If your travels take you through Invermere, British Columbia, be sure to stop at the **Backwoods Blues** music festival for authentic blues music and food. Tickets cover a weekend package chock-full of live bands including Kelly Jay of Crowbar, Mighty Joe Young and guests, The Overtones, Boogie Patrol and Randy Owen among others. Admission also includes dry camping for the weekend; 250-434-4444, horsethiefhideout.ca.

MAY 22-28 | Step back into the era of the Wild West at the



PHOTO: BEACHEVENTS

MAY 5

Kite enthusiasts of all ages are welcome to participate in the **Atlantic Coast Kite Festival** held on 17th Street in Virginia Beach, Va. The daylong event includes a Best Kite Contest (homemade kites judged on how long they fly and how well they're built), People's Choice contest (festival-goers get to vote on their favorite kites) and vendors selling various types of kites; 757-425-3111, www.beachstreetusa.com.

31st Annual Old West Days Celebration in Jackson Hole, Wyo., where visitors from all over the country come to listen to live music, take a peek at arts and craft vendor offerings, enjoy live theatrical entertainment, eat scrumptious food and watch an authentic rodeo show. There will also be a hawk and knife competition, and bow and arrow competition; 307-733-3316, jacksonholechamber.com/old_west_days.

MAY 26-27 | All are welcome at the annual **Doors Open Toronto** event. The event began as a millennium project when it launched in 2000, and has since attracted more than 2 million visitors eager to check out hundreds of buildings showcasing the most innovative of Toronto's architecture and design elements. Notable buildings include the Spadina museum, MaRS Discovery District, the RC Harris Water Treatment Plant and Justice Leadership Building at Humber College. The 2012 event and

building information will be released May 1 online at www.toronto.ca/doorsopen.

MAY 31 | To celebrate the promise of summer, the **Ithaca Festival** in Ithaca, N.Y., includes a variety of entertainment for families, couples and friends. The festival kicks off with a mile race followed by a

parade and festival concessions including hot dogs, hamburgers and fresh-squeezed lemonade. In the parade, giant dancing puppets, green tubas, dogs in costume, miniature Darth Vadars and other floats march down the street to the beat of a chainsaw marching band; 607-273-3836, www.ithacafestival.org/parade.

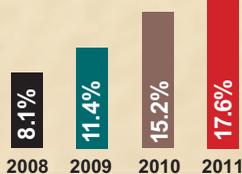


PHOTO: THE CITY OF TORONTO

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^{*}Source: Statistical Surveys, Inc. Calendar Year 2011.



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MORE SPACE FOR YOUR STUFF

Additional exterior storage is just one feature of **Thor Motor Coach's** Citation Class C motorhome. The Citation's Mega-Storage compartment brings the coach's total exterior storage capacity to 16 cubic feet in the 27SB model (24 cubic feet in the 29GB) while the oversized 36-inch-wide shower adds to the interior's residential feel. Additional features include glazed cherry or maple cabinetry, Onan MicroQuiet 4,000-kW generator, patio plus entertainment package, 32-inch HD LCD TV, 30-inch residential microwave, optional solid-surface kitchen countertops and Leatherette U-shaped dinette.

The bedroom offers a queen mattress along with a portable 19-inch LCD TV that can be removed and mounted to the exterior. The new-for-2012 Citation rides on a Ford chassis with 6.8-liter Triton V-10 engine; the gross vehicle weight rating (GVWR) of both models is 14,500 pounds. Base MSRP is \$98,663.

Thor Motor Coach, 800-860-5658, www.thormotorcoach.com

ECO-FRIENDLY SHINE

Be kind to the earth while also keeping your motorhome clean with a new line of eco-friendly appearance care products from **Eagle One**. Products in the company's EnviroShine line — All Wheel & Tire Cleaner, Tire Shine, Car Wash and Glass Cleaner — are made with environmentally safe ingredients that are non-toxic, biodegradable and free of harsh solvents, according to the company.

Clean wheels and tires in one step with EnviroShine All Wheel & Tire Cleaner. The foaming action penetrates and releases brake dust and road grime on contact for a deeper clean. For tire detailing, EnviroShine Tire Shine is simple to spray on and wipe off. The non-sling formula, according to Eagle One, means Tire Shine stays put and dries to a long-lasting, high-gloss shine. It is weather resistant and provides UV protection.

The Car Wash is said to be safe for all finishes and is formulated for better cleaning and to enhance a wax shine. The Glass Cleaner removes dirt and grime leaving a streak-free shine and it is safe for tinted windows. The EnviroShine line is available at auto parts retailers nationwide. Product prices range from \$4.49-\$6.49.

Eagle One, 800-832-6825, www.eagleone.com



REST EASY

Hours on the road mixed with sightseeing stops and activities galore will have most RVers longing for a good night's sleep. Weary travelers looking for a mattress upgrade now have another option: the RV Supreme Euro Top all-foam mattress from **Denver Mattress Hospitality and Recreation Nation** (a DEHCO company).

This RV-specific mattress is made of lightweight foam, is easy to install and comes with a five-year non-prorated warranty. The mattress is compressed, rolled and shipped, and when unpacked it expands to a 10-inch-thick queen or king bed. The RV Supreme is available at select RV dealerships and online through Recreation Nation's website. The RV Supreme is priced at \$599 for a queen, \$739 for the king. **Recreation Nation, 855-866-8877, www.recreationnation.me**

WOODALL'S RELEASES 2012 DIRECTORY

The latest edition of the Woodall's campground directory has been released and features more than 300,000 updates and changes from 2011.

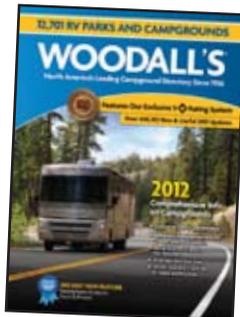
The 2012 Woodall's North American Campground Directory contains detailed listings for more than 8,000 privately owned and 6,000 public RV parks and campgrounds across North America. Included are listings of RV dealers, service centers and attractions. It also displays color state and provincial maps, at-a-glance charts showing parks that welcome big rigs, have Internet access (including Wi-Fi), welcome pets, and parks and campgrounds that are considered green-friendly, based upon the criteria of the National Association of Campgrounds and RV Parks (ARVC) in conjunction with "Leave No Trace."

Included in the 2012 edition is exclusive new editorial on "One Tank Trips," which guides readers to trips using only a tank of fuel. Also new is the use of QR codes allowing RVers and campers to con-

nect with the campgrounds, RV parks and resorts found in the directory. Located throughout the directory, these QR codes can be scanned by smartphones and will connect users to exclusive online content.

Woodall's also redesigned its Travel Section for 2012, filling it with tips and recommendations about local cities and attractions within each specific state.

In conjunction with the release of the new directory, Woodall's Publications is also releasing its list of top-rated, privately owned RV parks and campgrounds in North America through its 5W/5W rating system. Of the 8,000 privately owned parks listed in the 2012 directory, 363 parks earned the highest and most coveted designation in the Woodall's rating system. The 2012 North American Campground Directory is available online at www.woodalls.com for \$14.95.



News Briefs

Allison has introduced a new model within the company's 1000 Series transmissions, which increases the gross combined weight rating (GCWR) from 26,000 to 30,000 pounds. The 1350 transmission will provide end users with increased towing capability, according to Allison. Additionally, the 1350 is equipped with a park pawl. This means no air brake is required and the driver will experience ease of use comparable to a pick-up truck.

Kampgrounds of America's Ninth Annual Come Kamp & Care With Us Weekend event is set for Friday, May 11, and Saturday, May 12. Campers who stay as paying guests on May 11 will receive a free night of camping May 12 at any of the 400-plus participating KOA campgrounds. The event is a fundraiser to support KOA Care Camps, which is a network of 44 specialized camps that provide a summer camping experience for children undergoing or recovering from treatment for cancer. For a list of participating KOAs, go to www.koa.com/come-camp-with-us.

The RV Learning Center is offering scholarship money to sophomore, junior or senior undergraduates majoring in business, finance, economics, accounting or other RV-related subjects. The Center awards a \$2,500 scholarship annually and the application deadline is June 4. Applicants must have a 2.8 or better cumulative GPA and a 1050 minimum SAT score; complete a 500-word essay; submit a copy of their Free Application for Student Aid form; demonstrate the ability to fund a portion of their educational expenses on their own; and attend an accredited four-year college or university as a condition of receiving the scholarship. For more information, email info@rvda.org or visit www.rvlearningcenter.com.

The National Park Service is making visitor access improvements at Roosevelt Farm Lane at the Home of Franklin D. Roosevelt National Historic Site in Hyde Park, N.Y. The project includes construction of a permanent Roosevelt Farm Lane trailhead and 40-car parking area on historic Roosevelt lands at the south end of the Hyde Park Drive-In Theatre. A connecting trail from the new parking area to Roosevelt Farm Lane will be constructed and will include a small bridge and a boardwalk.

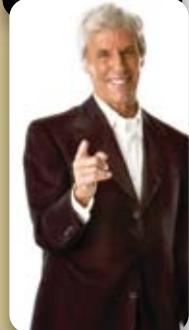
SHERYL CROW TO HEADLINE GOOD SAM LOUISVILLE RALLY

Louisville, Ky., is rolling out the welcome mat for The Good Sam Rally June 21 through 24, presented by DISH Network.

Rally attendees will enjoy the talents of Sheryl Crow as the Grammy winner is set to perform her many hit songs, which include "Every Day is a Winding Road" and "A Change Would Do You Good." In addition, award-winning composer and musician Burt Bacharach will tickle the ivories in Freedom Hall on Friday night. A living legend in the music world, Bacharach will perform his long line of hits, including "Anyone Who Had a Heart," "Walk On By," "I Say a Little Prayer," "Raindrops Keep Fallin' on My Head" and more.

The Louisville Rally will be held at the Kentucky Exposition Center, and the center's Freedom Hall offers ideal seating and acoustics for these performers. The Rally fee includes all four nights of entertainment and much more. Step out of your rig, hop on one of the shuttles and spend the day visiting the huge trade show with new RVs of all sizes. Browse the exhibit area for those accessories to complement your RV lifestyle and choose from hundreds of informational seminars that will help make your RV travels more enjoyable. The traditional Rally Dog Show will allow guests to bring their pups to show off their canine skills and personalities. There will be also opportunities to win hundreds of prizes worth thousands of dollars.

For more information, visit www.therally.com. The final Rally of 2012 will be held in Daytona, Fla., at the Daytona International Speedway, November 2 through 4.



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- Kansas Sports, Boat & Travel Show
- N.C. RV & Camping Show
- Atlantic City RV & Camping Show
- Colorado RV, Sports, Boat & Travel Show
- Rhode Island RV & Camping Show
- Virginia RV & Boat Show
- Summer Boat & RV Super Sale
- North Carolina Fall RV Show & Sale
- Big East Powersports Show
- Rocky Mountain Snowmobile Expo
- Fall Atlantic City RV Show
- St. Paul Ice Fishing & Winter Sports Show

For dates, times and
more information, visit

<http://www.gsevents.com/rv-shows.cfm>

Dish Network, Circle 124 on Reader Service Card

A PEZMERIZING MUSEUM

In 1927, Edward Haas of Vienna, Austria invented a small peppermint to combat bad breath. Initially named after the German word for peppermint, pfefferminz, but shortened to PEZ, the candy has been a favorite for nearly 90 years. By 1935, the demand for PEZ was growing so a Czechoslovakian factory was built to produce mass quantities of the candies. Oscar Uxa designed the first PEZ dispenser in 1948 to resemble a cigarette lighter in an effort to get people to stop smoking. One year later, the dispensers were featured at the Vienna Trade Fair. As PEZ-mania spread to the States, marketing changed to appeal to American children instead of adults wanting to kick their cigarette habits. Dispenser



PHOTO: BURLINGAME MUSEUM OF PEZ MEMORABILIA

designs evolved throughout the 1950s, starting a trend that has only gained momentum and innovation.

Today, PEZ is primarily known for its state-of-the-art, creative designs, often with pop culture nods including Mickey Mouse, Bugs Bunny, Batman, Kermit the Frog and Miss Piggy, to name a few. Each year, roughly 5,000 visitors come out to see 900 PEZ dispensers on display at the Burlingame Museum of PEZ Memorabilia located in Burlingame, Calif.

For more information, call 650-347-2301 or visit www.burlingamepezmuseum.com.

LIFE ON THE PRAIRIE

The history of the Gibbs Museum dates back to 1835 when Jane DeBow, a 6-year-old orphan, arrived in Minnesota after missionaries brought her there from New York to start a new life. DeBow grew up among Cloud Man's band of Dakota near Minneapolis, learning the way of housing, agriculture and family structure from their Native American influence. In 1849 DeBow married Herman Gibbs and the two bought 160 acres of land where they built their first home, a dugout sod house ("soddy"). Five years later, the Gibbs family built a larger farmhouse to accommodate their growing family. Throughout their remaining years, the Gibbs family added on to the farmhouse, building a parlor, a small room off the parlor, four bedrooms upstairs, a guest bedroom and quarters for six to 10 working men to sleep during growing season when they worked the Gibbs' fields.

Today, visitors can step into the past and see the Gibbs' farmhouse and a replica of the soddy house. The Gibbs Museum tour also includes views of a tipi, the tripod-based homes of the Dakota, and a walk through a preserved Dakota prairie with plentiful prairie grasses and wild flowers. Visitors will also see the Dakota gardens where the Dakota grew corn, beans, squash and sunflowers.

For more information, call 651-646-8629 or visit www.rchs.com. ♦



PHOTO: RAMSEY COUNTY HISTORICAL SOCIETY

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AUDUBON STATE HISTORIC SITE, LOUISIANA

ONCE THE HOME OF NATURALIST JOHN JAMES AUDUBON, OAKLEY PLANTATION IN ST. FRANCISVILLE IS STILL A HAVEN FOR NATURE LOVERS

It was in the lush forest setting of Oakley Plantation northwest of Baton Rouge, La., that John James Audubon lived as an art teacher while working on his book, *Birds of America*. The year was 1821 and *Birds of America* was to become Audubon's masterpiece.

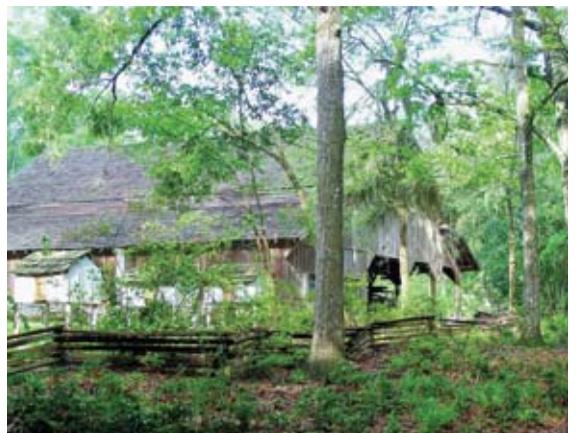
Writing in his journal, Audubon aptly described his setting: "The rich magnolias covered with fragrant blossoms, the holly, the beech, the tall yellow poplar, the hilly ground and even the red clay, all excited my admiration. ... and surrounded once more by numberless warblers and thrushes, I enjoyed the scene."

The James Pirrie family, owners of Oakley, had hired Audubon to teach drawing to their young daughter, Miss Eliza Pirrie. While the arrangement was short-lived and Audubon returned to New Orleans after just four months, he began or completed 32 bird paintings while at Oakley. Those paintings, along with 403 others, became *Birds of America*.

The canopy of beeches, poplars and oaks clustered overhead provided cooling shade as we drove the short distance to the visitor center at Audubon State Historic

Site. The verdant natural forest setting, with its many birds singing throughout the 100-acre site, inspired us as it did Audubon.

Although there is no on-site camping, Peaceful Pines RV Park and Campground is located just across the street from Audubon State Historic Site and offers 48



Located at the back of Oakley Plantation, the barn not only housed livestock but tools and supplies as well.

PHOTOS: LOUISIANA OFFICE OF TOURISM

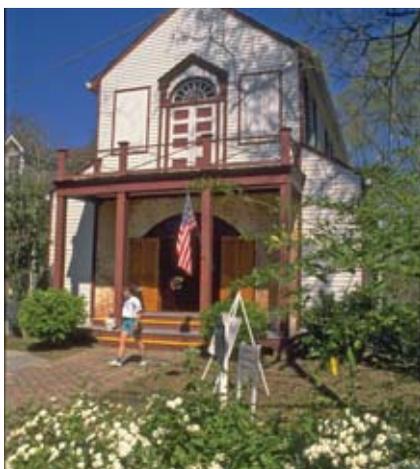
full-hookup sites, free Wi-Fi, a swimming pool, laundry facilities and a private fishing pond. Also nearby is the 10,000-acre Cat Island National Wildlife Refuge, which features birding, hiking and the state's largest cypress tree east of the Sierra Nevada.

We began our tour at the visitor center for an introduction to the site. Following the lead of a small informational leaflet, we headed down the original carriage path to the stately Oakley Plantation house. It predates the Classic Revival style shown in Southern plantation homes, reflecting its Colonial past through the West Indies approach of jalousied galleries, which welcome cool breezes, and a raised basement.

Standing at the base of the stairs, we sensed we were about to enter another world, the 1821 world of Audubon. After



A re-enactor participates in the West Florida Rebellion event at Audubon State Historic Site.



Audubon Market Hall is one of the stops along St. Francisville's annual Audubon Pilgrimage.

FOR MORE INFORMATION

AUDUBON STATE HISTORIC SITE

Entrance Fees: \$4 per person; free for seniors (62 and older) and children age 12 and under; 888-677-2838, www.crt.state.la.us/parks/iaudubon.aspx

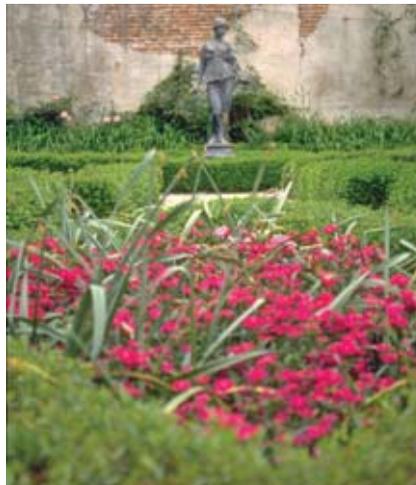
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passing through the door at the top of the stairs, we entered the rooms containing Colonial period pieces, their walls bedecked with vibrantly colored prints from Audubon's "Birds of America" series. Later, in the outside gallery overlooking the forest and formal garden below, it was easy to envision the naturalist/artist with his sketchpad in hand, taking notes and making drawings of birds.

For the visiting photographer, the formal garden created a foreground setting to capture Oakley House in all its splendor. Built around 1806, Oakley House stands tall at three stories and is filled with rooms restored to the late Federal Period (1790-



The gardens of Rosedown Plantation are another popular St. Francisville stop.

1830). For the garden enthusiast, St. Francisville is also home to what in the 19th century was one of the largest private gardens in the United States — the Rosedown Plantation State Historic Site.

Behind Oakley House, a walkway led us to the large detached plantation

kitchen, typical of the period. Using the old foundation, the large kitchen was reconstructed around the original chimney. It was here that foods such as wild game, mutton, beef, breads, pork, vegetables and even plum pudding were prepared.

Walking past the vegetable and herb garden, we came to two old slave cabins.

Records show that in the 1840s Oakley had about 250 slaves. Inside, the clothing, furniture, worn burlap sacks, tools and boards used for sleeping gave us a glimpse into a slave's life on the plantation. The second cabin contains copies of the original documents covering slavery at Oakley and the southern United States.

Experiencing the 100-acre site, it's easy to see how the wildlife, natural beauty and simplicity of a time gone by could

inspire Audubon to create the artistic works that would become his legacy. As we strolled through the grounds, soaking up the fascinating scenery, we found ourselves reading the imaginary pages that made our visit to Audubon so special. ♦

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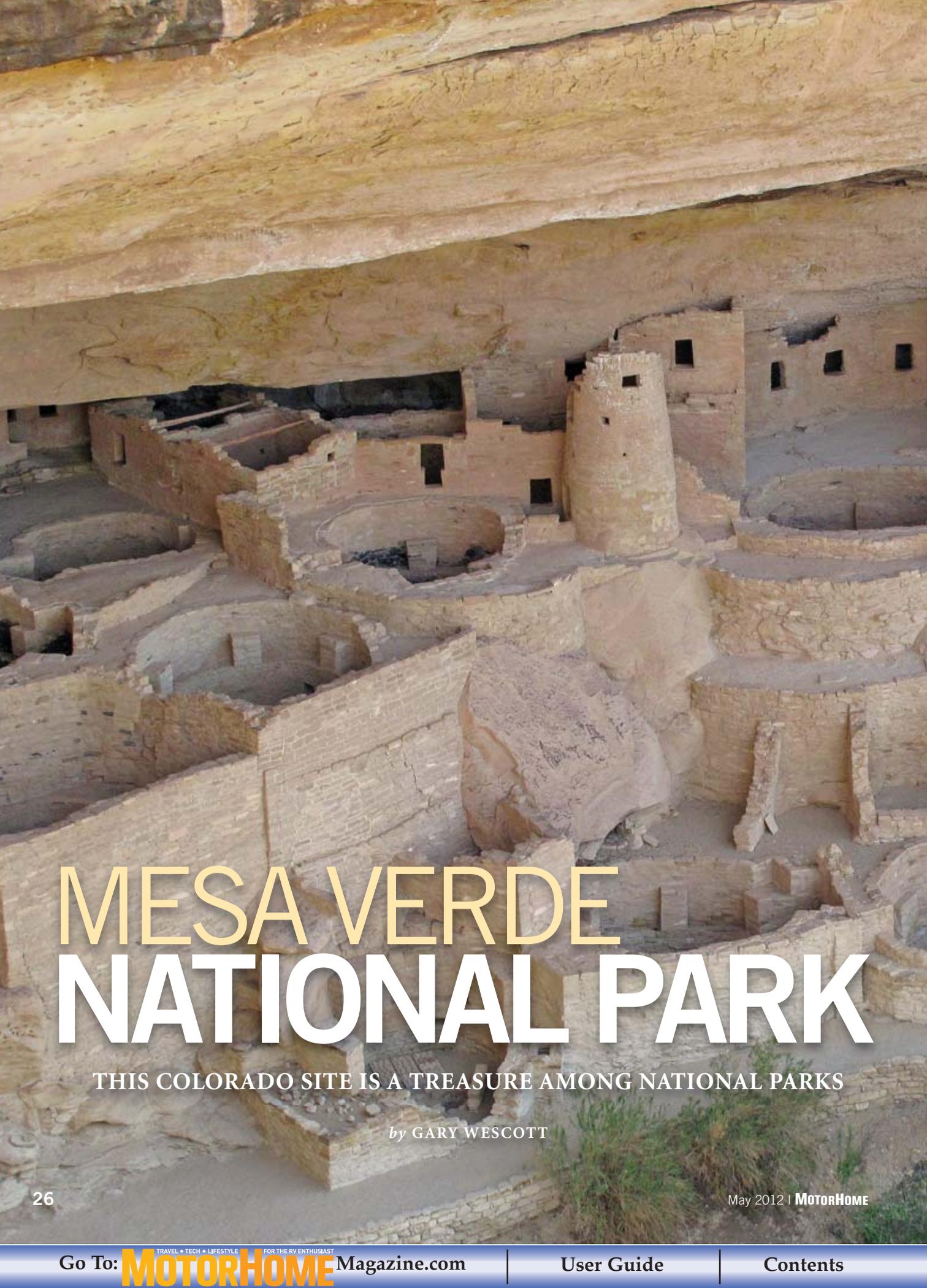
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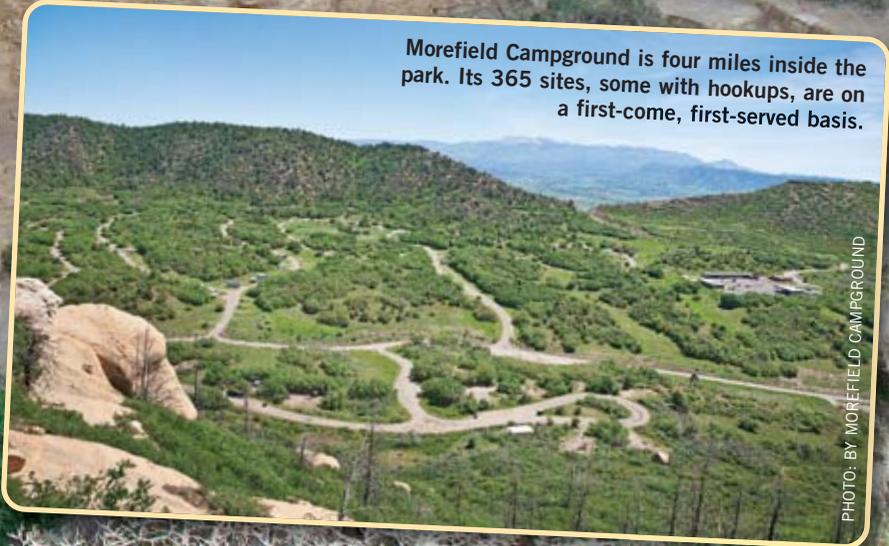
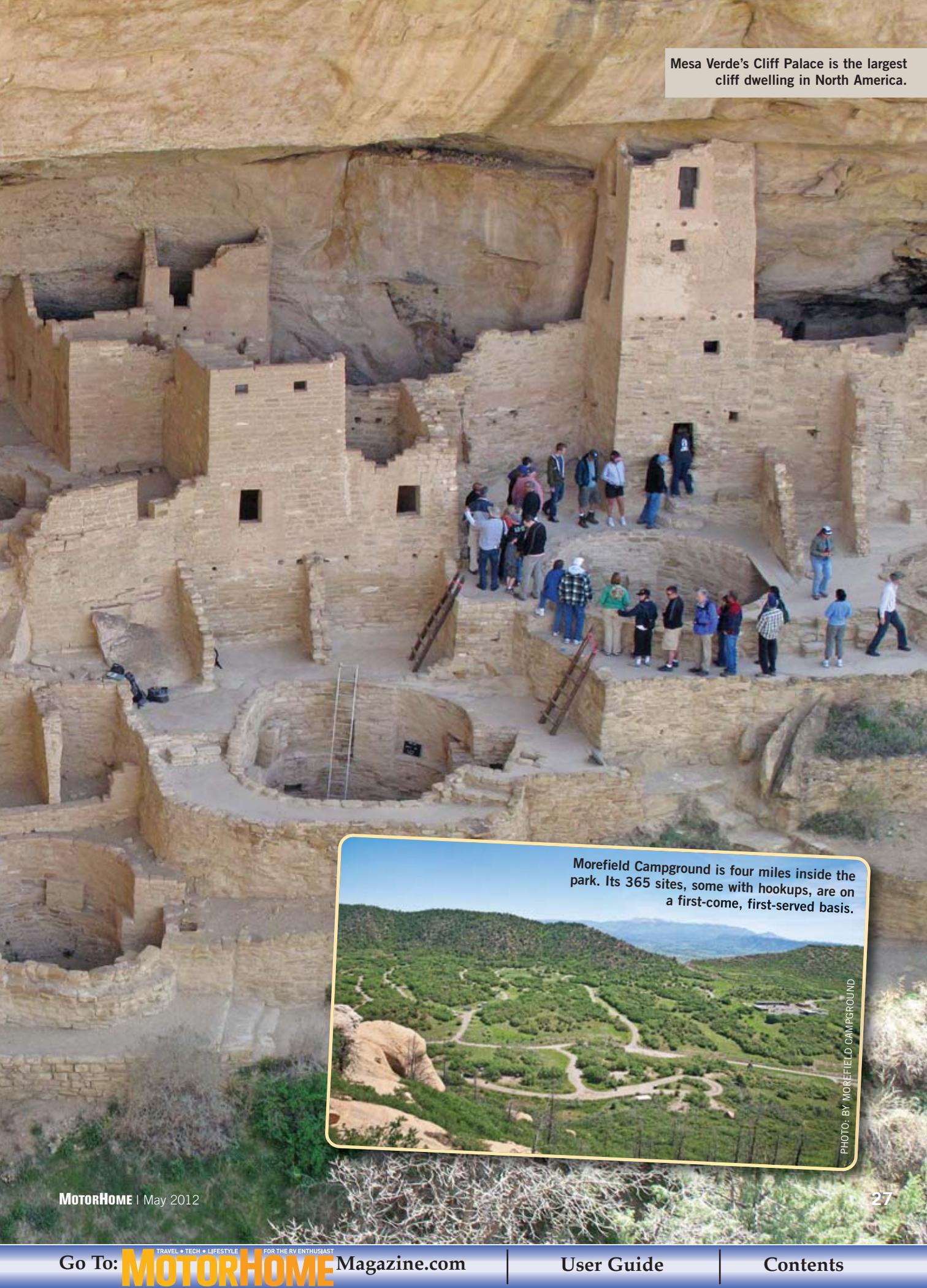


MESA VERDE NATIONAL PARK

THIS COLORADO SITE IS A TREASURE AMONG NATIONAL PARKS

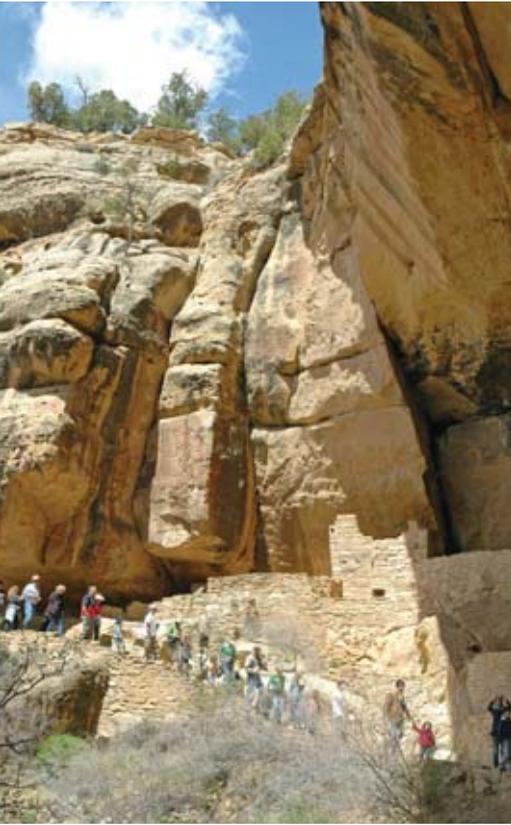
by GARY WESCOTT

Mesa Verde's Cliff Palace is the largest cliff dwelling in North America.



Morefield Campground is four miles inside the park. Its 365 sites, some with hookups, are on a first-come, first-served basis.

PHOTO: BY MOREFIELD CAMPGROUND



Top: A ranger-led tour through the Cliff Palace is enlightening. Above: These multi-story buildings were constructed perhaps a thousand years ago without tractors, motors or even a wheelbarrow.



The A & A Mesa Verde RV Park is the closest full-service campground to the park entrance.

Of all the amazing national parks in the West, Colorado's Mesa Verde is surely one of the most impressive. Mesa Verde National Park was established on June 29, 1906, by President Theodore Roosevelt to, as he put it, "preserve the works of man." It was the first national park of its kind. Today it is also a World Heritage Site. Recently voted by National Geographic as one of the top 10 places to travel in the world, Mesa Verde, Spanish for "green table," offers a spectacular look into the lives of the Ancestral Pueblo people, (sometimes called the Anasazi), who made it their home for more than 700 years, from A.D. 600 to A.D. 1300. With more than 4,000 known archaeological sites, the park has 600 cliff dwellings, some of the best preserved in the world. Basically, cliff dwellings refer to structures built within caves and under outcroppings.

Mesa Verde encompasses 81 square miles and preserves the greatest number of cliff dwellings in America. Obviously, we were not going to see this national park in one day. During our visit, early in the season, the Wetherill Mesa, which includes the Long House, the Step House and the tram to the Kodak House Overlook, were not open. We turned south onto the Chapin Mesa and stopped at the Far View Visitors Center where we purchased tickets for the ranger-led tour through the Cliff Palace. If you can see only one, this is the largest cliff dwelling in North America. The one-hour tour involves climbing five, 8- to 10-foot ladders, on a 100-foot vertical climb. Total walking distance is about a quarter-mile, round-trip. The tour begins at Cliff Palace

Overlook, an eight-mile, 20-minute drive from the visitor center.

Tickets are required and can be purchased at the Far View Visitors Center, the Morefield Ranger Station, the Colorado Welcome Center in Cortez and Chapin Mesa Archeological Museum. Tour-goers are advised that visits to the cliff dwellings can be strenuous. Trails are steep and uneven, with steps and ladders to climb, and are not recommended for those with heart or respiratory problems.

A WALKING JOURNEY THROUGH TIME

The tour down through the Cliff Palace was enlightening. We think of our society as being so advanced, yet these multi-story buildings were constructed perhaps a thousand years ago without tractors, mo-

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tors or even a wheelbarrow. These Ancestral Puebloans were farmers and hunters, and deer, rabbits and squirrels were plentiful. The pinyon and fragrant juniper, the same we walked through, were cleared for agriculture. Tops of the mesas were reached by hand-and-toe-hold trails pecked into the canyon walls. Try that with a basket of corn on your arm, we mused.

Another interesting ranger-led tour is the Balcony House. Also one hour long, it involves climbing a 32-foot ladder, crawling through a 12-foot-long tunnel and climbing up a 60-foot open rock face with two 10-foot ladders to exit the site. The tour begins at the Balcony House parking area, a 10-mile, 25-minute drive from the visitor center.

The Anasazi were skilled basket weavers. Some of the finest can be seen in the Chapin Mesa Museum. The baskets were used for carrying water, storing grain and even cooking. Later, about A.D. 550, pottery was introduced, and they became accomplished potters. Since the Anasazi kept no written records, their artifacts are the only link to understanding their interesting culture.

There are numerous hiking trails and self-guided tours to sites like the Spruce Tree House, the Puebloan Pithouse and villages. The Spruce Tree House, said to be the best-preserved cliff dwelling, is open year-round, however during the winter months it is available by free, ranger-guided tour only. This one-hour tour involves a 100-foot descent and ascent on a winding path. Walking distance is a half-mile round-trip. Tours begin at the Chapin Mesa Archeological Museum. During spring, summer and fall (early March to early November), the Spruce Tree House is available on a self-guided basis.

The pithouses date back to the first Ancestral Pueblo people who settled in Mesa Verde 1,400 years ago. They lived in small rooms sunk a few feet into the ground with a roof supported by timbers. These later evolved into kivas, the Hopi word for “ceremonial room.” A subterranean kiva remained at about 50 degrees Fahrenheit all year round. For the Ancestral Puebloans, it stayed cool in the summer, and only a small fire was needed to keep it warm in the freezing winter. Many of these are open for viewing, and a few are accessible by ladder. Some archaeological digs are protected from the weather.

Several of the park’s best hikes leave from Morefield and climb to spectacular views of surrounding valleys and mountains. Fortunately, most of the major cliff dwellings can be seen from the vista points off the paved roads, but we found it well worth the time to park and walk to some of the overlooks.

The Morefield Campground is four and a half miles from the park entrance. Its 365 sites, some with hookups, are on a first-come, first-served basis. There is a dump station, showers and other amenities. The campground is open from mid-May to mid-October. There are two other beautiful RV parks nearby. The A & A Mesa Verde RV Park is the closest full-service campground to the park entrance, and the Mesa Verde RV Resort is just a half-mile away.

As amazing as Mesa Verde is, still more puzzling is how this civilization disappeared. In the space of a generation or two, they vanished. Today, archaeologists are still pondering why and where they went. The rock walls and pottery shards cannot speak. As one ranger noted, “The Anasazi left so little in their passing, while our civilization is leaving tall buildings of cement and steel, polluted rivers and lakes, huge waste dumps and hundreds of thousands of miles of blacktop.”

“Perhaps,” he continued, “millennia from now, the true measure of the success of a civilization will not be how much it left behind, but how little it left behind.” ♦



Top: A few of the kivas — Hopi for “ceremonial room” — are accessible by ladder. Center: A few archaeological excavations are protected from the weather but open to the public. Above: Some kivas are open for viewing and have been well restored.



A MAINE EVENT

LOBSTER LOVERS WILL ESPECIALLY ENJOY A COASTAL MAINE ADVENTURE

by BOBBIE HASSELBRING

They're not pretty, but they are delicious. *Homarus americanus*, the lobsters found off America's northeastern coast, are some of the most delectable foods on the planet. Nowhere can you find more tasty — and bargain-priced — lobsters than the sweet, soft shell ones found at the Maine Lobster Festival in Rockland. So that's where we're headed.

We begin our lobster safari in Boston. With gas prices soaring, we hop an inexpensive flight from Portland, Ore., to Boston and rent a motorhome.

Once in Beantown, our mouths are already watering and we begin sampling lob-

ster pizza at jail-turned-restaurant Scampo, lobster scampi at iconic Union Oyster House, and lobster rolls at Boston's new Island Creek Oyster House. With taste buds primed, it's on to the lobster festival.

Maine's coastline is rugged and spectacularly beautiful. Just 12,000 years ago, glaciers retreated and sculpted the state's rocky coast into 3,000 miles of bays, coves, inlets and peninsulas. Roads on these coastal fingers tend to be winding, two-lane affairs choked with traffic during high season. We avoid the crowds by driving north on Interstate 95, a toll road (\$2.50 for RVs) that zips us to Kennebunkport.

Like many quaint villages in Maine, Kennebunkport has plenty of cafés, galleries, souvenir shops and ice cream parlors. The problem is that the streets are tiny and not RV-friendly, especially for larger rigs. The solution would be a dinghy vehicle or shuttle. The Red Apple Campground offers a free shuttle to and from Kennebunkport. After snuggling the motorhome into a shady space, we shuttle into town. When we're ready to return, we just call.

Later, we enjoy another perk — fresh, hot Maine lobster delivered right to our motorhome by the park's owner. Sitting at our picnic table, we experience our first

PHOTOS: ANNE WEAVER



Captain Tom of the Finest Kind 1 lobster boat checks this lobster's tail to see if she's a breeding female.



Seen from the top of the Ferris wheel, the large tents are where they serve up to 20,000 pounds of lobster every year at the Maine Lobster Festival.



Sunset at Cape Porpoise turns lobster boats into a picture postcard.

soft shell summer lobster in the rough (boiled). It's incredibly sweet and delicious.

The next morning, we explore the coast, finding breathtaking views around every corner. Some shorelines are carpeted with marble- to grapefruit-sized rocks. Others are crowded with square boulders the size of refrigerators, marching like penguins to the sea. Still others slope like immense, striated sheets of pulled taffy and plunge into the surf. Lighthouses wink tirelessly from rocky outcrops. Sailboats, powerboats, lobster and fishing boats ply the waters and everywhere small, multi-colored lobster trap buoys bob in the water. It's as though we've stepped into a painting.

Around one corner, we're surprised to see a large home with American and Texas flags flying. This is Walker Point, home of former President George Herbert Walker and Barbara Bush.

On Interstate 295, we exit for Perkins Cove, with its wharf, shops and restaurants and the Finest Kind 1 lob-

At Salt Water Farm Campground in Thomaston, campsites overlook the St. George River and are only minutes from the Maine Lobster Festival.



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Boiled Lobster	12.00		Single Lobster Dinner	14.00	
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Boiled Clams	10.00		Smoked Shrimp	7.00	
Boiled Crab	10.00		Smoked Mussels	10.00	
Boiled Fish	10.00		Shrimp Dinner	18.00	
Boiled Fish	10.00		Corn on the Cob	1.50	
Boiled Fish	10.00				
Boiled Fish	10.00				
Boiled Fish	10.00				
Boiled Fish	10.00				



Summer Maine lobsters are especially sweet and their soft shells are easy to open.



Hungry visitors line up for boiled lobster (“in the rough”) at the Maine Lobster Festival.

ster boat. Captain Tom greets us as we board this working lobster boat outfitted with benches so we can watch the action. We motor into open water and spot a purple and white buoy that marks our first lobster trap. Each lobsterman is allowed 800 traps and they use distinctive buoys to flag them.

An electric winch hauls up the trap and two wiggling lobsters. The average take is one lobster for every three traps so we’re doing well. Captain Tom measures the lobsters from eye to carapace (where the tail begins), ensuring they’re between 3.5 and 5 inches. Breeding females are marked with a tail notch and thrown back. This lobster fishery, managed by lobstermen, is so healthy that fishermen bring in record catches.

While we motor from trap to trap, first mate Stephanie fills us in on the natural history of the lobster. Within 40 minutes, we’ve collected a dozen lobsters (the company sells them to locals and tourists) and it’s back to port.

All this lobstering has made us hungry. We head to MC Perkins Cove for what turns out to be the perfect lobster roll —

Lobster makes everything better. This lobster pizza was delicious.

big pieces of lobster barely moistened with mayonnaise and stuffed into a soft, lightly grilled bun. As we watch the sunset transform the water and the boats into a picture postcard, we’re content.

ON TO THE LOBSTER FESTIVAL

The following day back on I-95, we get off at the Freeport exit to stretch our legs and check out the L.L.Bean outlet stores. Freeport has become a shopping mecca and has plenty of RV parking. There’s also a nice campground here (Freeport Village Campground) with easy freeway access, but we’re lobster-bound so we keep going.

We take the U.S. Highway 1 exit toward the coast and, as we pass through Thomaston just outside of Rockland, we spot the Salt Water Farm Campground sign. We don’t have reservations, but this off-the-beaten-path park overlooking the St. George River proves just right — grassy, quiet and within a couple of miles of terrific lobster. (Most Maine RV parks are open from mid-May to mid-October.)

We cruise into Rockland, park the rig on the street and follow the crowds to the lobster festival. With carnival rides, live entertainment and vendors hawking every-

thing from cotton candy to sunglasses, the festival looks like any county fair. That is, except for the flatbed trailer with the giant plastic lobster, the otherwise ordinary looking people sporting lobster claws on their heads, and a huge tent where people happily don plastic bibs and chow down on boiled lobster.

The Maine Lobster Festival (Aug. 1-5, 2012) is an all-volunteer event celebrating the spiny crustacean. They boast the largest lobster cooker in the world and, during the five-day festival, cook more than 20,000 pounds of the clawed creature.

At the food tent 20 people are ahead of us, but the line moves quickly as volunteers fill orders for one-, two- or three-lobster dinners, complete with corn, roll and cups of melted butter. We gather our one-lobster dinners (a bargain at \$14), grab a seat overlooking the water and happily dig in.

It's the best lobster we've ever tasted. In the summer when lobsters come close to shore to mate and molt, the shells are soft and the meat is especially sweet. Visiting Maine is the only way to experience this summer treat.

There's more to explore in Rockland, including the Farnsworth Art Museum that houses the largest collection of paintings by the Wyeth family, well-known American

artists. For a small additional charge, visitors can tour the Olson House, the lonely farmhouse made famous by Andrew Wyeth's iconic *Christina's World* painting.

Another must-see in Rockland is the Maine Lighthouse Museum. To guide ships along its treacherous coastline, Maine built 70 lighthouses. Docent Marla Rogers lived at Owl's Head Lighthouse as a youngster and spins tales about the hardships lightkeepers' families faced. She leads us through exhibits of brilliant Fresnel lenses, maps and photographs. Afterward, we drive a short distance to Owl's Head Lighthouse for terrific views and a few photos.

NORTH TO ACADIA

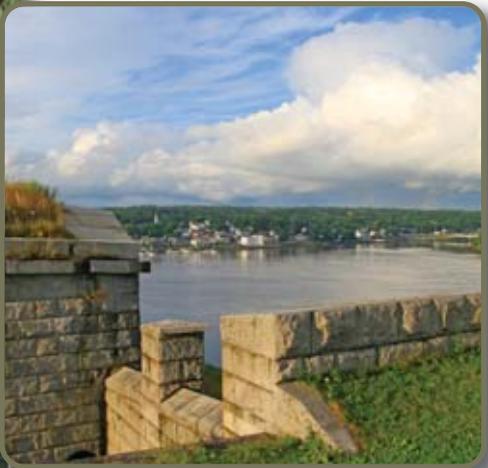
The following day, we head north to Acadia National Park, the first national park created east of the Mississippi and one of the most visited. We climb slopes covered in birch, fir and pine, and crest bare-rock granite summits, stopping to stare awestruck at mountains plunging into the sea.

We hike the park's famous carriage roads. In the 1930s, wealthy philanthropist John D. Rockefeller Jr. financed a 50-mile network of gravel carriage trails, 17 granite bridges and two gate lodges. As we stroll the shady paths, we're reminded why this area has long been a vacation favorite.



Above: Birch trees, like these in Acadia National Park, grow large in Maine. Native people used the trees' bark for shelters and canoes. Below: This camper in Boothbay Harbor has found a shady spot to park her motorhome.





Left: Two KOAs just outside of Acadia National Park offer all the amenities and a free shuttle into the park. **Above:** Fort Knox is one of many forts that protected coastal Maine.

FOR MORE INFORMATION

- ACADIA NATIONAL PARK**
207-288-3338,
www.nps.gov/acad
- BAR HARBOR KOA OCEANSIDE**
888-562-5605,
www.koa.com/campgrounds/bar-harbor-oceanside
- BAR HARBOR KOA WOODLANDS**
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- MAINE LOBSTER FESTIVAL**
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The park offers two no-hookup campgrounds in forested settings. However, at two nearby KOAs, you can have all the amenities and use the free shuttle to the park. Maybe it's because Clayton the Lobsterman boils lobsters for KOA campers that we decide to camp here.

The next morning, we head to Bar Harbor, one of the state's iconic fishing villages. With its clapboard New England-style homes and inns, shady central park square, shops galore and on-the-water locale, Bar Harbor is cute. It's also jammed with tourists and RVs aren't allowed to park except in designated areas. I buy a lobster sleep shirt and we make a quick escape.

It's a two-hour drive between Bar Harbor and Camden, but we meander. Just outside Ellsworth, we scamper over granite ramparts at Fort Knox. We pick up Maine wild blueberries at a roadside stand. While Albert boxes our berries, his wife Maudine tempts us with free slices of blueberry pie. We also spot a bakery sign and bump down a gravel road to the Bread Box Bakery where we load up on cinnamon bread for breakfast. Half the loaf disappears before we're back on the highway.

At Searsport, we stop at Penobscot Marine Museum, an impressive, three-acre complex of turn-of-the-century buildings filled with boats and maritime memorabilia. The Fowler True-Ross House is an original captain's house loaded with antiques, including a piano with mother-of-

pearl keys.

Just before Camden, we turn at Camden Hills State Park and climb up Mount Battie for a commanding view of the town and Penobscot Bay from the stone viewing tower. Mount Battie and Acadia National Park are the only two places in Maine where the mountains march straight into the sea and it's an impressive sight. The park's big, forested campsites convince us this is another place to call home.

We're on the road early, keenly aware this is our last day. We pull into Boothbay Harbor, another quintessential waterside village. Yearning for one more taste of lobster, we breakfast at historic Spruce Point Inn where I order lobster Benedict – two farm fresh eggs, big chunks of lobster and lemon hollandaise sauce on English muffins.

Heading toward Boston, we pass through the famous shipbuilding town of Bath and make one last stop at the Maine Maritime Museum. We only have time for a few exhibits, including the massive sculpture of the Wyoming, the largest wooden sailing vessel ever built. Rain has settled in and we brave the wet for a quick tour of a working cod schooner that gives us a glimpse into the hard lives these fishermen lived.

As we climb back in the motorhome, we take one last look at Maine's coastline. We know we'll be back. We've barely tasted coastal Maine and summer means more sweet, soft shell lobster. ♦

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THE ITALIAN JOB

THERE'S A LOT TO LIKE ABOUT THE NEW FIAT 500. BUT IS IT AMORE?

by CHRIS HEMER

Fiat's return to the U.S. market was met with as much trepidation as it was anticipation. Not merely because the company would be riding across the Atlantic on the shoulders of its most recent acquisition, the Chrysler Corp., but because of its lackluster history in the United States during the '70s and '80s. The company withdrew from the U.S. market in 1984, and for the most part, hasn't been missed.

Fiat aims to change all that with its Fiat 500, a miniscule two-door hatchback that oozes cuteness. A long-standing favorite in Europe, the U.S. market 500 is built in Mexico with a 1.4-liter four-cylinder Fiat-designed engine assembled by Chrysler employees in Michigan. It's about as Italian as an Olive Garden chef, but to be fair, most cars on the market today are "world cars" with assembly plants and suppliers located around the globe. The question now is, how will the Fiat brand be accepted this time around? And how good of a car is the 500?

We can't predict the former (although sales at press time are well below expectations), but we can tell you that there's a lot to like about the Fiat 500. First and foremost to motorhome owners, the manual transmission version is dinghy towable, and the procedure couldn't be simpler: put



it in neutral and insert the key into the ignition to unlock the steering wheel. The 500 is also very light at only 2,363 pounds, so it's towable behind just about any motorhome.

The Fiat 500 may be a subcompact as far its EPA vehicle class is concerned, but unlike some other cars in the segment, a low price point wasn't Fiat's primary objective. Like the popular MINI, the 500 is part of a growing trend toward small, premium cars aimed at people who don't want to sacrifice style and comfort for something that's economical and



From left: The Fiat's interior is a delight for the eyes, with a body-colored dash and chrome trim everywhere. The seats were some of the most comfortable we've sat in, but the instrument cluster could use some improvement. Fiat maintains that the 500 seats four, but when we placed the front passenger seat in a position that was comfortable for two normal-sized adults, there wasn't enough legroom even for a child. The 1.4-liter MultiAir engine produces 101 horsepower, which is just enough for spirited driving in this lightweight subcompact. It's also good for an EPA-estimated 30 MPG city, 38 MPG highway.

easy to park. As such, the base model, the Pop, starts at \$15,500, the Sport at \$17,500 and the Lounge at a heady \$19,500 (there are also special models like the \$23,500 Gucci model, the \$22,000 Pink Ribbon and the upcoming performance-oriented Abarth at \$22,000). That's a lot of green for a little car, but when you look a little closer you'll start to understand why.

For one thing, the 500 is offered in 14 exterior colors with 14 available interior color and material combinations — try finding that in another subcompact. And it comes standard with features like air conditioning, cruise control, power windows/locks and power heated exterior mirrors, leather-wrapped steering wheel, AM/FM/CD radio and a BLUE&ME hands free communication system with iPod, USB and MP3 interface. All models also include safety features like four-wheel disc brakes with ABS, electronic brake-force distribution (EBD) and brake assist, electronic stability control, traction control and seven air bags.

The Sport model adds a rear spoiler, unique front/rear fascias, 16-inch alloy wheels, sporty red brake calipers, sport-tuned suspension, fog lamps and Bose premium audio. Lounge, as its name would suggest, leans more toward luxury, with a standard six-speed automatic transmission (not towable), fixed glass roof, bright exterior accents and climate control.

For our test, we chose the Sport model in "giallo" (yellow), because it offers some great driver-centric features but still comes standard with the five-speed manual transmission required for dinghy towing. Our car was equipped with the available Safety and

Convenience Package (recommended), which adds automatic temperature control with micron filter, a compact spare tire (instead of a tire repair kit), an alarm and heated front seats, for just \$600. It also had the optional TomTom navigation unit, which plugs into a port on top of the dash, for \$400. This brought the price of our tester to \$19,000, not including tax and license.

As we've mentioned, the 500 is undeniably cute, but thankfully that isn't the theme of the whole car. The turn signal doesn't make silly noises, it doesn't have a flower vase and the horn sounds surprisingly like a horn. No, Fiat's mission with the 500 was to engage the driver, and we think it has succeeded. For example, the seats are among the best we've ever sat in. They're very comfortable, but also supportive and well bolstered. Eye candy is everywhere, from the exterior-colored dash accent to the chrome shift knob and door handles. And the ergonomics are excellent — almost everything is within a finger's reach of the steering wheel and shift knob.

The only thing we really didn't like was the instrument cluster, a sort of circle within a circle arrangement that makes it difficult to discern the tachometer/speedometer needles at a glance. To make things worse, the dash lighting and needles are almost the same shade of orange, so the problem is compounded during night driving. We'd also forgo the optional navigation unit; it's distracting if you're tall, and blocks your view if you're short. A better (and cheaper) choice would be one of the many portable navigation units you can stick to the windshield.

While 101 horsepower doesn't sound

like a lot in a small car with a manual transmission, it gets the job done, especially when paired with the sport suspension and Pirelli rubber offered by the Sport model. We put this car through its paces on some of the most challenging roads in North America, namely the Santa Monica Mountains north of Los Angeles, and found the chassis dynamics to be well sorted. The 500 Sport has the kind of suspension tuning that American and Japanese manufacturers haven't quite mastered (especially in a car of this size) — it's both firm and comfortable, and the handling is very neutral.

Moreover, while Fiat did a good job with the chassis, there is room for improve-

SPECIFICATIONS

BODY STYLE: TWO-DOOR HATCHBACK
ENGINE: 1.4-L SOHC 16-VALVE INLINE FOUR-CYLINDER
HORSEPOWER: 101 BHP @ 6,500 RPM
TORQUE: 98 LB-FT @ 4,000 RPM
TRANSMISSION: FIVE-SPEED MANUAL
WHEELBASE: 90.6"
OVERALL LENGTH: 11' 7.6"
OVERALL HEIGHT: 4' 11.8"
CURB WEIGHT: 2,363 LBS
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ment. For example, the clutch travel is too long and the take-up is too sudden, which makes the car difficult to drive away smoothly. The brakes, while very strong, are also grabby; better pedal feel would be appreciated. And the power steering could use some work; it felt sticky on center, and was overly sensitive at freeway speeds.

The Fiat 500 is the first subcompact this writer would gladly take on a long

drive, as the suspension and seats make it a very comfortable tourer. There's adequate cargo space under the hatch, and the rear seats fold down to accommodate more gear if necessary. Good thing, too, because they're not much good for passengers. Despite Fiat's claim that the rear seat is designed to be comfortable for two adults, this is, for all practical purposes, a two-seater. Two adults could conceivably ride

in the back, but they'd have to be very small and the ride short.

The Fiat 500 is an ideal dinghy vehicle, and a great little car that will be perfect for the small but growing premium small car audience. If it can overcome Fiat's less-than-stellar reliability record and last like its Japanese counterparts, it will likely find a permanent home in the United States this time around. ♦

DINGHY DELIGHT

When considering a car for dinghy towing, the common considerations are light in weight and towing ease — and the 500 has both in spades. Weighing in at just more than 2,300 pounds, it's about as light as cars get today, and as mentioned in the main text, preparing it for towing is as easy as putting a key in the ignition and shifting the transmission into neutral. But how difficult is it to set up for towing behind a motorhome?

As it turns out, this is one of the easiest cars we've ever set up.

After removing the front fascia, it's a simple matter of unbolting the bumper and bolting on the EZ Twistlock Series "Hidden" bracket from Roadmaster. This bracket features front arms that can be easily removed when not towing — simply twist the front arms 90 degrees to remove unsightly bracketry when you're ready to hit the town.

Follow along with us and see how easy it is to set up a Fiat 500 (Pop or Sport models only) with the Roadmaster EZ Twistlock Series bracket.



A) The first step is to remove the four Torx-head screws that secure the top of the front fascia.



B) Four more Torx-head screws secure the front fascia from underneath the car, just ahead of the air dam.



C) Torx head screws in front of each fender must also be removed.





D) The screws at the leading edge of the inner fender liner must be removed so that the liner can be pulled back. This will provide access to the headlight harnesses, which must be unplugged before the fascia can be removed.



E) The fascia still felt tight after we had removed all of the fasteners, so we did some investigation before going any further. Turns out these two 10 mm bolts behind the air dam (not mentioned in the instructions) were hanging up the assembly, so they were loosened.



F) Here, the front fascia is carefully pulled loose. This requires some patience, because the plastic clips that hold the fascia in place are easily broken.



G) With the front fascia removed, the metal bumper is revealed. This will be removed and replaced with the Roadmaster adapter.



H) A ratchet and socket are used to remove the bumper fasteners on each side.



I) Here, the Roadmaster adapter is lifted in place and bolted in for trial fit.

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J) The front fascia is hung in place with a few fasteners so the grille can be marked for cutting. Note the receiver tube of the adapter just behind the grille.



K) Once confirmation is made that the adapter will mount using the existing holes, it is bolted into place using the Fiat's original bumper hardware.



L) The Fiat's plastic honeycomb grille is trimmed using a pair of tin snips.



M) The finished adapter installation is very clean with the receiver tubes barely visible behind the Fiat's grille.

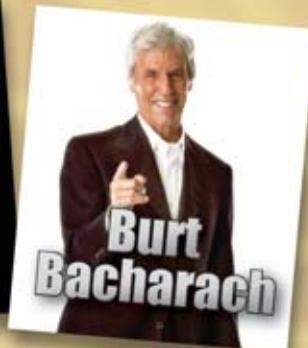
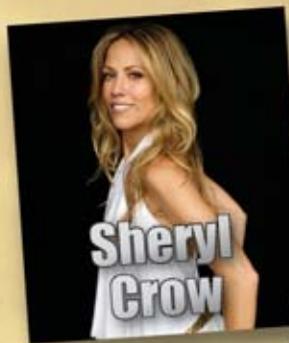


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N) The Twistlock brackets and safety cables are installed next, then the brackets for the crossbar/quick-connect hardware are bolted into place. Rather than using brand-new brackets, we opted for brackets that we already had from an earlier project. ♦



The finished installation looks great and is ready for towing behind a motorhome.

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LIGHTEN UP

FIND THE PORTABLE LANTERN THAT FITS YOUR TRAVELING NEEDS

by E. DON SMITH



If you plan on spending much time traveling in your motorhome, there will eventually come a time when you need battery-powered lighting. It could be a sunset trip to the lake that lasts a little longer than expected or an outside dinner at a roadside picnic table. Although flashlights are a common item in most travelers' arsenals, they are not very good at lighting a large area and they usually require you to hold them in your hand. In those circumstances nothing replaces a freestanding battery-powered lantern.

If you are a longtime motorhome enthusiast, you will likely remember the old fuel-powered lanterns of the past. Not only

did they provide a lot of light, they also lasted a long time on a single tank of fuel. The performance of these fuel-based lanterns has been the standard for decades, but recently the lighting industry has made great strides in replicating the performance of fuel-based lanterns without the danger and hassle of carrying fuel in your coach.

The new battery-powered lanterns are a reasonable competitor to the old fuel lanterns in terms of lighting and battery life, and they offer a lot of choices with regard to battery type as well as light source. Many of these battery-powered lanterns use energy-saving fluorescent or LED lights instead of the old, inefficient incandescent

bulbs of the past. Some offer built-in rechargeable batteries while others stick to conventional disposable alkaline batteries for their power. Some even allow the use of both, making them quite convenient.

While they work great for providing light while eating at a picnic table or fishing off a pier after dark, these lights are not a replacement for normal AC-powered lights. Though they do provide enough light for many activities, uses such as reading are best left to other light sources. For example, a normal 100-watt light bulb in a household lamp offers more than 1,200 lumens, which is more than triple the brightest light we tested.

In order to help you pick the best bat-

PHOTOS: E. DON SMITH

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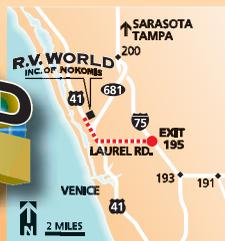
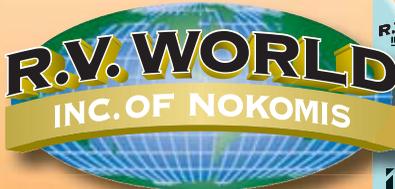
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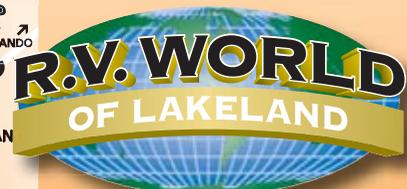
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tery-powered lantern for your motorhome needs, we went to a variety of local and online stores, bought some of the most com-

mon models and put them to the test. Since everyone has different needs when it comes to battery-powered lighting, it is hard to proclaim a real winner and loser, but after you look at our rankings in various categories you should be able to pick up one

or more of these that will suit your needs.

To help you sort through the various features of each product we ranked them with a 1-5 star rating in several categories, with 5 being the best score. The first category is brightness. Since that is the primary function of a lantern, the ability to deliver a



Back row from left to right: Cabela's Bugproof LED, Cabela's 12-LED with Remote, Coleman 8D Pack-Away, Coleman CPX 6, Coleman 4D Rugged LED. Front row from left to right: Rayovac Sportsman Mini, Energizer Weather Ready 360-Degree, Rayovac Sportsman 300 Lumen, Coleman 4AA Pack-Away.

LANTERN RATINGS

MODEL	Cabela's Bugproof LED Lantern	Cabela's 12-LED Lantern with Remote	Coleman CPX 6 Easy Hanging LED Lantern	Coleman 4D Rugged LED Lantern	Coleman 4AA Pack-Away Lantern	Coleman 8D Square Pack-Away Lantern	Energizer Weather Ready 360-Degree Area Light	Rayovac Sportsman Mini Lantern	Rayovac Sportsman LED 300 Lumen Area Lantern
PRICE	\$59.99	\$39.99	\$34.99	\$29.99	\$22.99	\$29.99	\$19.99	\$14.99	\$24.99
BRIGHTNESS	3	3	4	4	2	5	1	3	5
BATTERY LIFE	5	5	3	4	1	2	5	3	3
SIZE	3	3	3	3	5	2	4	5	4
FEATURES	4	2	3	3	2	2	3	4	4
DURABILITY	5	3	3	3	3	3	1	4	4
EVEN LIGHTING	3	1	3	4	2	5	5	5	5
VALUE	2	2	3	3	3	2	2	4	5
TOTAL	25	19	22	24	18	21	21	28	30
Claimed time on high	175	500	65	7	5	12	29	65	72
Actual hours with alkaline	210+	210+	81	168.25	8.5	11.6	210+ in LED mode	72	157
Actual hours with rechargeable pack	N/A	N/A	8	7.5	N/A	N/A	N/A	N/A	N/A

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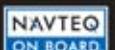


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lot of light over a large area is important. The second category is battery life. Longer is better and some of the models are more efficient than others. LED and fluorescent bulbs use less power than incandescent bulbs, making them ideal for this application. We cut off battery testing at 210 hours and as all lights approached the end of their life, light output was greatly diminished. The next category is size. Since storage space in a motorhome is limited, smaller is usually better when it comes time to pack a lantern. We also evaluated the various features of each light. Some have remote controls, nightlights and high/low and strobe settings that make them more useful than those that don't. We also evaluated the durability of each model in an attempt to predict its life on the road. We also noted a large difference in the way each lantern diffused the light, so we also rated them in their ability to project a smooth, even light over a large area. Finally we rated them all in value, which is an overall score of performance relative to cost.

While this is not an article on batteries, we need to mention one important point regarding the use and storage of any battery-powered device. Alkaline batteries account for about 80 percent of all batteries sold in the United States and they are widely used because of their long storage life and ability to produce a lot of power. Most alkaline cells are not intended for recharging so it's best not to recharge them unless you are using a rechargeable version. Another concern of alkaline batteries is when they are completely discharged they will leak and this usually destroys the device they are in. Therefore, if you plan on keeping a backup lantern, make sure you do not completely discharge the batteries or else you may have to buy a new lantern.

Our scores add up with the two Rayovac Sportsman lanterns on top and for good reason. They are well thought out; provide good, even lighting and good battery life at a low price. However, for outdoor usage don't overlook Cabela's Bugproof LED lantern. The yellow light will be much less attractive to bugs and this model also scored well on our tests. If maximum brightness is your primary concern, then get the Coleman 8D Pack-Away lantern. It has 390 lumens and is great at providing light. If you plan on using your lantern a lot, the Coleman rechargeable models should be considered. Over the long run recharging the CPX 6 battery will be far less expensive than buying a lot of D batteries.

CABELA'S BUGPROOF LED LANTERN

\$59.99; www.cabelas.com

Includes two settings of bug-free yellow lights and one white LED setting. Uses four D-cell batteries that are contained in a bottom O-ring sealed compartment. Manufacturer claims up to 175 hours of battery life and 160 lumens, and 1,400 hours on the amber nightlight setting. It is rated at IPX-7 waterproof, which is 1 meter for 30 minutes, and it floats in water.

PROS: Three lighting modes, waterproof, solid construction, long battery life due to LED bulbs.

CONS: Expensive and somewhat uneven light pattern.



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CABELA'S 12-LED LANTERN WITH REMOTE

\$39.99; www.cabelas.com

Includes a remote control that operates light up to 35 feet away. Uses four D-cell batteries in an O-ring sealed compartment. Manufacturer claims up to 500 hours of continuous run time, which is 21 days. Only has one brightness setting.

PROS: Very long battery life and has remote control.

CONS: Light pattern is uneven.

COLEMAN CPX 6 EASY HANGING LED LANTERN

\$34.99; www.coleman.com

Manufacturer claims 200 lumens of light for up to 65 hours; 155 hours on low setting. Has a convenient strap for hanging. Includes housing for four D-cell batteries or you can buy the optional CPX 6 charger for \$24.99 that includes the battery and 120-volt AC as well as a 12-volt DC car charger. Recharger connects on the bottom so it can't be charged while standing up. Has a high/low switch.

PROS: Strap for hanging and smooth bright light.

CONS: On/Off button hard to locate. Rechargeable batteries don't last long and are not included.



COLEMAN 4D RUGGED LED LANTERN

\$29.99; www.coleman.com

Uses four D-cell batteries or Coleman's CPX optional rechargeable batteries. Manufacturer claims 190 lumens and up to 14 hours of run time on rechargeable batteries. Recharger connects on the bottom so it can't be charged while standing up. High/low switch.

PROS: Includes rechargeable battery and 12-volt DC/120-volt AC cords. Smooth, bright light.

CONS: Rechargeable battery life is short.

COLEMAN 4AA PACK-AWAY LANTERN

\$22.99; www.coleman.com

Powered by four AA-cell batteries, which are included. Coleman claims 105 lumens. Very small and collapses for even smaller storage. Includes high/low switch and SOS mode.

PROS: Very small and easy to store, plus batteries are included.

CONS: Light pattern is very uneven.



COLEMAN 8D PACK-AWAY LANTERN

\$29.99; www.coleman.com

Coleman claims 390 lumens on high with a run time of 12 hours on high and 24 hours on low. Requires eight D-cell batteries. It collapses for easy storage but is still large and heavy because of eight D batteries. High/low switch.

PROS: Very bright light with a smooth, even pattern. Also collapses for easier storage.

CONS: Requires eight D batteries and is heavy. It is still a large light despite collapsible base.





ENERGIZER WEATHER READY 360-DEGREE AREA LIGHT

\$19.99; www.sears.com

Includes three D-cell batteries and a small LED keychain light. There is a

new all-LED version on the market, but the tester contained incandescent and LED bulbs. Includes carrying handle and hook on the bottom for hanging. Has several modes including a nightlight.

PROS: Small, yet produces good and even light. All battery testing was done in LED mode, which allows for great battery life.

CONS: It required a lot of adjusting and tweaking/cleaning the battery terminals to get it working on several occasions so we are concerned about long-term durability.

RAYOVAC SPORTSMAN LED 300 LUMEN AREA LANTERN

\$24.99; www.rayovac.com

Claims 300 lumens on high. Uses three D-cell batteries, which are not included. Has a three-position switch including high, low and strobe. Claims 72



hours on high and 150 hours on low.

PROS: Dome is removable for wider light radius. Includes a hook for hanging. Bright and even light in a small package at a reasonable price. Best scores of the group!

CONS: It's not fair to call this a con, but because of the brighter bulb the battery life is lower than other models tested.



RAYOVAC SPORTSMAN MINI LANTERN

\$14.99;

www.rayovac.com

Very small. Includes three AA-cell batteries and removable globe as well as a hook for hanging. Claims 16 hours on high and 35 hours

on low. Also has high/low and strobe switch and is water resistant.

PROS: Small size and smooth, even lighting makes this a great choice if a small light meets your needs.

CONS: AA battery yields shorter life. ♦

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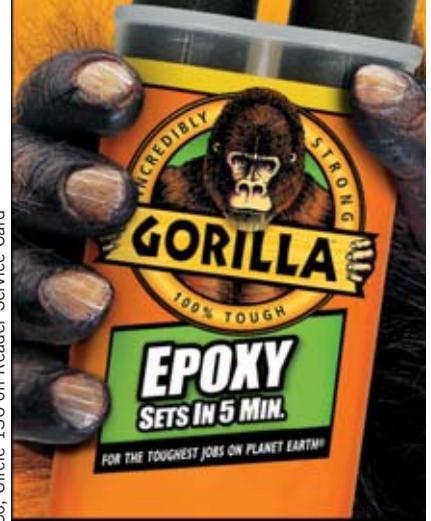
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During a very successful six-year run of the popular Seneca, chassis builder General Motors ceased production of its Topkick/Kodiak medium-duty chassis, forcing Jayco to find a suitable replacement. With past and present owner input in mind, the Seneca went on a complete redesign hiatus for much of 2011, allowing Jayco to reassess its popular Class C inside and out. The results reflect not just a simple refresh of an existing line, but a new generation for the Seneca.

The most significant visible change is the new Freightliner M2/106 Business Class cab and chassis and standard full-body paint package. The 340-horsepower 6.7-liter Cummins ISB diesel, with engine brake, turns an Allison six-speed overdrive automatic transmission creating a powertrain combo that offers improved performance and handling. The chassis increases the gross vehicle weight rating (GVWR) by 3,500 pounds and provides 5,000-pound increase in the gross combination weight rating (GCWR) over the first generation Senecas.

Drivers will appreciate the large 2,500-square-inch windshield and aerodynamically sloped, front-hinged hood that not only provides a panoramic highway view but also retains excellent under-hood maintenance access. Unlike large, open Class A cockpits, the Seneca offers a more intimate cabover cockpit layout, which creates the confidence-inspiring feeling of driving a smaller vehicle. Some of the driver amenities include a 55-degree wheel cut, heated side-view mirrors and side-view cameras, anti-lock brakes, and a keyless cab and coach entry, all standard features.

Inside the popular 36FK floorplan is a curbside forward



galley with an amidships entry. The galley offers a 10-cubic-foot double-door refrigerator with icemaker — an optional (\$1,129) 12-cubic-foot four-door model with icemaker is also available — flanking pantry cabinets, three-burner recessed range, an oversized oven and a 1.4-cubic-foot microwave. Rounding out the galley is a stonecast twin basin sink, which is set into the Corian solid-surface countertop material on the sink base cabinet.

Opposite the galley, the cabin's streetside slideout holds the facing dinette and pneumatic sofa bed combo or the more popular, but optional (\$1,421) dinette ensemble providing a single twin-size sleeping position. The ensemble creates a striking interior design further refined with one of three new interior décors and the medium-tone Washing-



BIG SCREEN SECURITY

MOR/RYDE MOUNTING BRACKETS
MAKE INSTALLING A LARGER FLAT-SCREEN TV
PRACTICAL FOR MOTORHOME TRAVEL

by BILL AND JENN GEHR



Whenever I walk into a motorhome, I immediately look for the TV. Perhaps it's an instinctual, "man cave" response, but the older I get, the smaller the screen becomes, leaving a strong desire to upgrade to a bigger TV. Our rig was outfitted with a 37-inch LCD TV from the factory, which seemed adequate until I checked out my friend's stately 46-inch screen as we entered his living area.

Without hesitation, we purchased a new 46-inch LED TV with all the bells and whistles. Buying the TV was easy; finding a way to mount it without compromising stability and safety became a much more stressful process. After unsuccessfully attempting to mount the TV with home entertainment brackets, we contacted MOR/ryde and came up with hardware perfectly suited for RV use — stout enough to be secure even while driving on poorly maintained roads.

The majority of wall-mounted TV brackets on the market are designed for stationary use, which precludes supporting relatively heavy flat screens in an environment that equates to seismic activity. Not only do the MOR/ryde mounting brackets take into consideration the stresses of being on the road, they provide mounting versatility necessary inside a motorhome. The company has a number of mounting systems that articulate for multiple viewing positions, but for our setup a fixed mounting bracket worked the best.

Prior to installing the MOR/ryde TV mount you should carefully consider the mounting area and make sure the structure can accommodate the addi-

Top left: MOR/ryde's Rigid Wall Mount is designed for flat-screen TVs up to 50 pounds, and uses VESA mounting patterns for the TV bracket. Left: Once installed, the 46-inch LED TV fit nicely in its allocated space and snug up against the back wall.

PHOTOS: JENN GEHR



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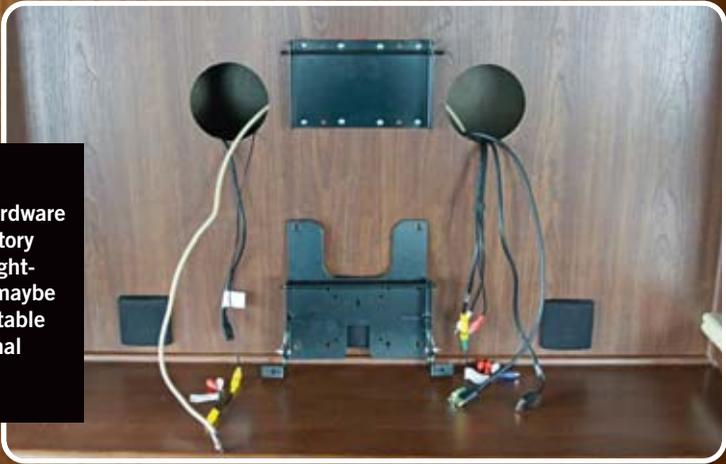
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This is a typical TV mounting kit for home use. It is not strong enough to withstand the rigors of motorhome use, so instead we chose the Rigid Wall Mount kit from MOR/ryde.

The original mounting hardware from the factory was pretty light-weight and maybe not even suitable for the original 37-inch TV.



The MOR/ryde wall plate mounted conveniently using screws that were driven into the wood backing. HDMI cables reduced dramatically the number of necessary hookup wires.

The adapter plate is screwed into the back of the TV using M8 bolts, which may be difficult to find at a local electronics store.



tional weight, which is five times that of your particular TV, plus the mounting bracket. The structure inside your motorhome may require the installation of additional reinforcements to handle the weight and the appropriate fasteners.

We began the mounting process by removing the stand from the TV and installing the adapter plate. This MOR/ryde adapter plate (model TV1-008H) does not come with the wall-mount bracket and must be ordered separately to fit the size of your TV. MOR/ryde offers adapter plates in three sizes and many specialty TV mount options.

Once the parts were gathered, the two halves of the mounting bracket were separated before the TV mount kit (model TV1-002H) could be bolted to the back of the TV. This process takes a little trial and error to find the optimum mounting position and to center the TV in the allocated mounting location on the wall or cabinet opening/structure.

Using the appropriate screws included in the kit, the other half of the mount is permanently attached to the wall. Set the TV near the wall mount area and attach the audio and video cables as well as the power cord. We highly recommend switching to HDMI cables because this high quality, single cable supplies both audio and video, simplifying the component connections.

With the help of another person, hang the TV onto the permanent part of the wall mount. To finish, just insert and tighten the two bolts that join the two halves of the wall mount. Turn on the TV and step back to check out your handiwork.

After 2,500 road miles in our RV, the MOR/ryde TV mount has kept our new big screen safe and secure. In addition to the overall enjoyment of having a larger screen, the new LED TVs are lighter and use far less energy, which is important when powering off an inverter when hookups are not available.

The MOR/ryde Rigid Wall Mount Kit includes bolts, washers, spacers and three different length screws for metal and wood applications. The suggested retail price for the TV1-002H mount is \$67.29; the adapter plate is \$41.39. Both products come with a one-year warranty. ♦

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Cummins, Circle 111 on Reader Service Card

COMPUTER CADDY ▼

My Itasca Sunstar has no provision for storing things next to the driver. I took a few measurements, made a cardboard mock-up for fit and had my neighbor build me a suitable stand for my computer, etc.



The stand weighs about 5 pounds and is made of 3/16-inch-thick plywood and is held together with glue and staples. I painted the wood with two coats of Deft clear wood finish. The top is covered with anti-slip material held in place with clear adhesive silicone. The bottom shelf is raised 1 inch because a flat surface would tend to wander around easier on the carpet than the edges of the sideboards. The front and left sides have patches of 1/4-inch-thick by 1-inch-square rubber "bumpers" to prevent the wood surface from scratching the wall. It measures 12 inches wide at the front, 9 inches at the back, 2 feet long and 19 inches high.

Now I have a place for my cold drinks, sunglasses, camera and most importantly my Dell Latitude D600 mapping computer with DeLorme Street Atlas USA 2011. The hole in the forward end is for the 12-volt DC power plug visible there. I also added a removable plastic tray (out of an old refrigerator) in the bottom of the stand, which I use as a container for carrying miscellaneous stuff.

I may in the future paint the stand to match the décor of the interior.

LUTHER STRUVE | POCATELLO, IDAHO

UNDER COVER ▼

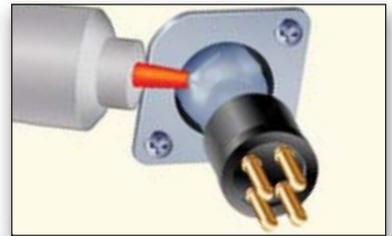
With the carpeting in our motorhome going right up to the entry steps and into the cockpit area the wear pattern has started to show. So, after a good cleaning and fluff up, I purchased two plastic covers — the kind of carpet protector or chair mat used in offices — from my local stationary store. I cut the two pieces to cover the area and now the carpet is always clean.



DONALD SCHULTZ | NORTH ROYALTON, OHIO

ELECTRICAL WATERPROOFING ▼

I use a dielectric grease compound each time I work on electrical connections that are exposed to water, road salts or corrosive environments. My towed umbilical cord is protected on male and female ends, and I've never had electrical contact problems. It improves electrical performance during adverse conditions such as rain, fog, salt spray and temperature extremes. Just fill any voids and coat connections with the grease, then assemble. There are several manufactures of dielectric grease available at a variety of stores. I purchased the grease from an electrical supply house. The grease is a bit messy, but it's worth a couple of paper towels.



DANNY LINDSTROM | SALINAS, CALIF.

EASY, BREEZY

The best fan can't ventilate a bathroom if fresh air can't get in to replace the air that is exhausted. In my rig, with the window closed because of weather, the only entry was under the door and the fan just whirled away while moving little of the air.



To improve airflow, I cut a hole in the lower part of the door and attached return air grilles purchased from a home improvement store to each side. My door is hollow, with thin veneer over a wood frame. I was able to make neat cuts with several strokes of a box knife.

First make a smaller exploratory hole to determine the construction of your door, and the grille size you should use. I chose nominal 12-inch grilles that are actually 13 3/4-by-13 3/4-inch overall, with 11 1/2-by-11 1/2-inch openings. Be sure the openings on both sides of the door are exactly opposite each other.

Glue wood spacers inside around the edges for reinforcement and to provide backing for the grille screws. Use screws short enough so that they won't touch. For privacy, install the grilles with the louvers angled down on both sides of the door.

Now, with a vent open elsewhere in the RV, my bathroom stays fog free. The furnace does a better job of heating it, too.

JIM ANDERSEN | AMITY, ORE. ♦

WE WANT TO HEAR FROM YOU!

Quick Tips, MotorHome's monthly column of useful, handy and simple tips by fellow RVers, is looking for submissions. Please send your favorite do-it-yourself ideas to: MotorHome Quick Tips, 2575 Vista Del Mar Drive, Ventura, CA 93001, or email letters@motorhome magazine.com. Be sure to include any photos, illustrations or drawings, if necessary. If your tip is selected for publication, you'll receive \$35.

ILLUSTRATIONS: BILL TIPTON

techsavvy

HANDS-ON | HOT LINE | COACH & POWERTRAIN

HANDS-ON | by KEVIN LIVINGSTON

UNDER PRESSURE

Easy-to-use pressure cooker makes motorhome meals a snap

It seems that regardless of the destination or the time of the year, RVers manage to somehow revolve their activities around food. How many times have you met with fellow motorhome owners for breakfast before heading off to an activity, already having made plans for yet another meal before the last bite was consumed? Let's face it, RVers love to eat. I can't imagine there's a motorhome out there without a barbecue stashed in a storage compartment.

But not all meals have to be prepared on a barbecue. One cooking tool seldom seen in motorhomes is a pressure cooker, mainly due to sheer size. While not exactly diminutive, the Can Cooker is destined to change the way we cook in our motorhomes. The Can Cooker is a pressure cooker, which means it uses steam inside a sealed container that cooks the contents quickly, relying on a specially designed and regulated lid. Now campground chefs can have the benefit of slow-cooked foods without the slow cooking times.

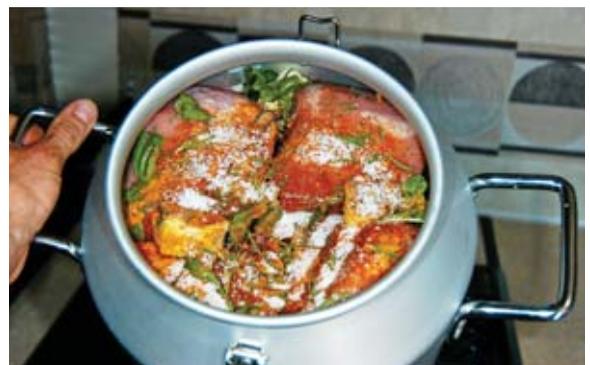
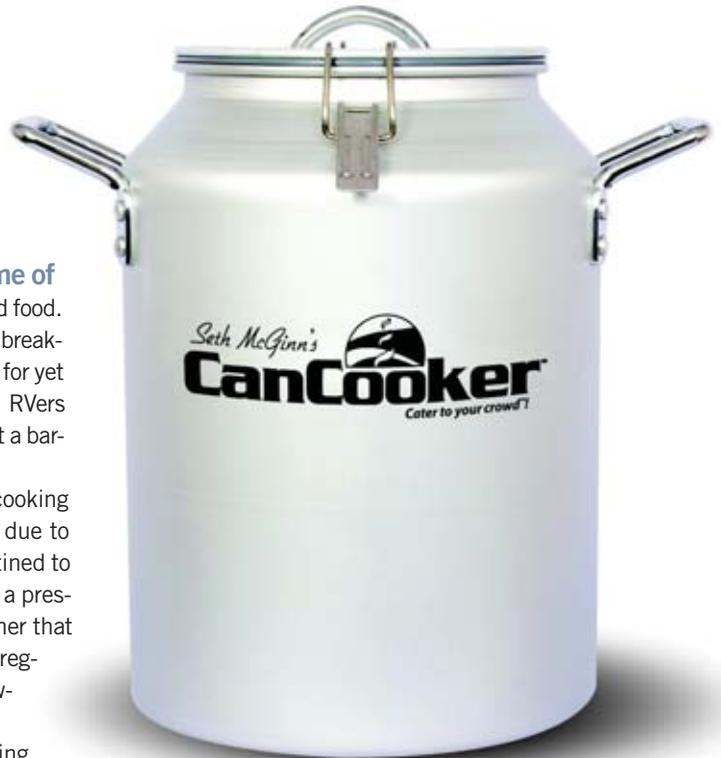
Most cookers of this nature are restricted to stovetop heating and are certainly not rated for something like an open campfire. However, the makers of Can Cooker created a device that combines the best ideas of pressure cooking, while allowing usage on just about any heat source.

Can Cooker is a simple yet incredibly sturdy pot constructed of anodized FDA-grade 1060 aluminum that's combined with features like riveted heavy-duty stainless handles and a high-temp silicone gasket to seal the lid. It has a capacity of 4 gallons and is 14 inches tall and measures 10 inches in diameter; weight is not an issue. This has got to be hands down one of the easiest cooking items to use. Just find a recipe that suits your taste, prep the ingredients, throw them in the can and cook for the appropriate time — usually within an hour. Basically, once you see steam, you know the cooking process is on its way. Steam usually appears in 10 to 20 minutes, depending on how much food you load into the pot. The company offers an optional rack for the bottom of the cooker, and for \$14, it's a good investment. Also, it's suggested that the user spray the interior of the pot with a non-stick cooking spray before using to prevent food from burning and sticking, which should also result in quicker cleanup.

The Can Cooker is a bit bulky and may take some thoughtful packing to keep it aboard your motorhome. However, if you want quick, hassle-free cooking and meals large enough to feed your own small army, the Can Cooker is the hot ticket.

The standard Can Cooker sells for \$89.99 and comes with a handy storage bag. If you buy the cooker with the bottom rack, it's \$99.98, which saves you a few bucks over buying the items separately.

Can Cooker Inc., 877-844-2772, www.cancooker.com ♦



PHOTOS: KEVIN LIVINGSTON

Tire Trouble

BELIEVING HE HAD A VALID DAMAGE CLAIM AGAINST AN EMERGENCY ROAD SERVICE PROVIDER AND ITS AFFILIATES, A READER ASKED HOT LINE TO GET INVOLVED. HE WROTE:

Last January, we were heading south to Texas for the winter in our 1986 Newell. Everything was going well until we got to El Dorado, Kan., where we encountered blowing snow. We pulled into a Walmart to park for a few hours and ride out the storm, but when it was time to leave the engine wouldn't start.

I called Family Motor Coach Association's Emergency Road Service for help. The customer service representative took our information and said they would call us back, but they never did. About 90 minutes later we received a call from the tow truck driver at Auto House Towing in Hutchinson, Kan. He said he was running late because of the storm. Two hours later he arrived, but he wasn't able to get underneath our motorhome to take out the driveshaft so it could be towed. Four hours later, a mechanic from Auto House showed up. He took the driveshaft out, hooked us up and towed us 45 miles to a repair center in Wichita.

The next morning, our coach was ready and we were back on the road. We later felt a bump, bump, bump coming from the front end. It was so bad that we couldn't go faster than 30 MPH. We stopped at the first rest area and saw that both front tires had been flat-spotted down to the fabric.

We called Strickland Road Service, which was able to locate one tire and install it for \$826.62. We found another tire and had it installed for \$558.05.

I contacted FMCA's Emergency Road Service and Coach-Net, the company that FMCA contracts with to provide the service, and was told that they carry no responsibility and that I would have to deal directly with Auto House. I think our coach should have been moved with a low-box trailer, rather than a wrecker, and that way we wouldn't have had any of these problems.

I hope Hot Line can help me reclaim these costs.

CHARLES TAYLOR | WELDON, IOWA

Taylor's plea was forwarded to FMCA with Hot Line's request for further consideration of his problem. A company representative, and later Taylor, responded as follows:

I received word from Coach-Net today that its vendor relations director has decided, after reviewing the information again, to pay Mr. Taylor the full amount for the tires. I apologize for the delay. Please let me know if you have any questions, or need further information.

**KATHY GEBHART
MEMBER RECORDS COORDINATOR
FAMILY MOTOR COACH ASSOCIATION
CINCINNATI**

Thank you very much for your help with my ordeal with Coach-Net and Auto House Towing. I received a check from Coach-Net for the full amount of the tire replacements (\$1,384.67). I question whether I would have accomplished this settlement without Hot Line's help.

C.T.

MUFFLER MISUNDERSTANDING

Frustrated with his attempts to get the manufacturer of his motorhome's chassis to recognize what he believed to be a valid warranty claim, a reader asked Hot Line to get involved. He wrote:

I would like Hot Line's help in obtaining reimbursement from Workhorse Custom Chassis for replacement of two mufflers on my 2009 Itasca SunCruiser at 17,082 miles. In addition to the SunCruiser, I have owned five motorhomes and all had more than 17,082 miles on them when sold and I never replaced any exhaust system parts. Workhorse manufactured the chassis in May 2008, Winnebago installed the body in August 2008 and I



bought the coach in January 2009.

Last April, I noticed both of the mufflers had rusted through. I contacted Workhorse and was directed to take my motorhome to Maudlin International Trucks in West Ocala, Fla. I confirmed the need for repair, but was informed by the service manager that Workhorse would not pay to replace the mufflers. I did not have time to appeal Workhorse's decision, so I had the mufflers replaced for \$470.

This motorhome has been housed well away from the coast and has never been exposed to chemical treatment, salt, etc., as contended by Workhorse in its letter of refusal. It is more likely that the manufacturer of the muffler used inferior material or the material was subjected to the elements prior to manufacture. I think I should be reimbursed the cost of the muffler replacement.

**EVERETT PERRIN
CRESCENT CITY, FLA.**

Hot Line forwarded Perrin's complaint to Workhorse Custom Chassis with a request for help. A company representative responded as follows:

This is in response to your letter regarding Everett Perrin. Thank you for contacting Workhorse Custom Chassis and forwarding Mr. Perrin's correspondence. We appreciate the opportunity to have reviewed the situation regarding the muffler repairs. Workhorse has authorized reimbursement in the amount of \$470 to Perrin as requested for the muffler replacements. Payment will be mailed within three to four weeks.

Again, thank you for the final opportunity to review this matter further. I sincerely appreciate your efforts in helping us to resolve any Workhorse customer concerns.

**DEBRA ANDERSON
REIMBURSEMENT COORDINATOR
WORKHORSE CUSTOM CHASSIS
TROY, MICH. ♦**

TO CONTRIBUTE TO HOT LINE, please refer to Contact MotorHome, on page 10.

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User Guide

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Problems Using Biodiesel?

During a trip in our motorhome from North Dakota to Iowa we filled the tank with biodiesel and within 100 miles the engine started acting as if it were running out of fuel. I pulled over, shut off the engine, restarted it after 10 seconds and then it ran fine again. I had to do this a number of times until we got home. When I checked, the stored trouble code indicated a fuel problem (injectors, filter possibly). I had run the tank low, so I filled it with plain No. 2 diesel, took a 400-mile trip, and it ran perfectly.

Could a batch of biodiesel cause this sort of problem? Also, could a tank of plain No. 2 clear up the gunky injectors and/or filter? Thanks for your help.
MARLAN AND BARB HAAKENSON
VIA EMAIL

You didn't mention what model engine you have, the percentage of biodiesel you used or what kind it was. Any fuel can be contaminated; I suspect you had some water in the fuel. You should remove the fuel filter and pour it out into a clear jar to see what's in it. Then install a new filter, fill it with clean fuel and bleed the air from the system. Also, carry a spare new filter in the coach.

GRADE BRAKING

I have a 2006 Winnebago Suncruiser motorhome with a gas engine and a grade brake switch to the left of the instrument cluster. Would you please explain how the grade brake works? Also, would the grade brake affect a Brake Buddy? I used the grade brake intermittently coming down a lot of mountains for a couple of hours and the Brake Buddy started sounding several different audible alarms. After a quick stop and retesting the unit, all was fine with no more alarms.

SANDY CIMAROLLI
WOLVERINE LAKE, MICH.

The grade-braking system is part of the Allison transmission, and its primary purpose is to downshift to provide en-

gine braking to heavy vehicles on steep downgrades, to reduce overheating and wear on the brakes. The system overrides the gear selector position and selects the next lower gear automatically. The transmission's software monitors several factors before directing a downshift: throttle position, brake application (brakes must be applied to activate grade braking), vehicle acceleration/deceleration, gradient/load and vehicle speed.

The transmission control module (TCM) calculates the effect of the added mass of a trailer or towed vehicle based on the effects of it on downhill speed and required braking. Your auxiliary braking system may be applying too much stopping power. Ideally you want the dinghy vehicle to neither push nor pull on the tow bar during braking, but rather to match the decelerating motorhome's braking.

CHARGING CHAGRIN

In my motorhome, when the generator has been on for more than an hour, the battery monitor still indicates it's not charged. I opened the two caps on one battery and I can only see water way down near the bottom. Should there be electrolyte up higher? I think that's why the batteries don't seem to be charging.
CARLA BAINES | SAN DIEGO

Typically, generators don't have good battery charging circuits, and take a long time to partially charge batteries, usually through the motorhome converter. Your results will depend on the kind of generator, the kind of converter, the kind of batteries and the age of the batteries. If the batteries are lead/acid batteries, as long as the water/electrolyte is above the tops of the plates, no harm is done. But if the level drops below the plate tops, the batteries will be weakened until they're unable to hold a sufficient charge. It's quite possible that your batteries have already become sulfated and won't take a full charge anymore. I suggest you have them tested by an RV shop, and also check charging voltage. Something simple, such as corroded battery and ground connections can also prevent proper charging.

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MOUSE-AWAY TIP

I have an idea that might help motorhome owners who have a problem with mice getting inside their coach while it's parked. I bought two 10-foot rolls of aluminum flashing material 20 inches wide from a hardware store. I then cut them in strips 10 inches wide and wrapped them horizontally around the bottom of the tires. Then I cut off the excess material, leaving only enough to overlap and put three sheet metal screws in each one to hold them together. You must make sure there is nothing hanging down elsewhere for the mice to climb up on. The 10-inch width of the aluminum may vary depending on the clearance under the inside of the wheels. The aluminum is too slick for them to climb up and I haven't had a

CONTINUED ON PAGE 62



IKON

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mouse inside our motorhome since. In my case it was a simple matter since I park on a concrete slab. If you park on gravel or dirt all you have to do is work the aluminum into it slightly.

BRUCE TYSON | DENVER

Thanks for sharing that tip, Bruce. It's sure to help some of our readers.

FRYING FORD

I have a 1994 Fleetwood Pace Arrow with a Ford F53 chassis, 7.5-liter fuel-injected gas engine with 72,700 miles. The motorhome has been running hot, especially during climbs. The temperature needle moves to the maximum of the range almost to red, but hasn't gone in the red because I turn on the heater. I live in the San Francisco area where the temperature is not very hot. I have had the thermostat and radiator replaced, and the head gaskets were checked by performing a dye check and it checked out good. The engine oil is clean (not milky), the coolant level is OK and the engine doesn't smoke. There are no leaks. I am scared to

drive it long distances and am not sure what to do. Would you please help?

B. KHAN | VIA EMAIL

The most likely culprit (since you changed the radiator) is the fan clutch. It should roar when it kicks in under load when hot. If you don't hear it, replace the fan clutch.

SWITCHING TO SYNTHETIC OIL

I am planning to replace the conventional mineral oil in my motorhome's gas engine with synthetic oil. The engine has about 80,000 miles on it. I've heard that mineral oils and synthetic oils are compatible. Is this true?

HAROLD CAMPBELL | CHICAGO

The term synthetic engine oil is generally used for lubricants formulated with polyalphaolefin (PAO) base oils, and PAO is compatible with conventional mineral base oils. But there's a hitch. PAO base oil does not dissolve additives well, so it's usually mixed with esters such as diesters and/or polyolesters. Ester-based oils used in conjunction with PAO base oil have

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strong detergent qualities. They will scrub varnish and sludge deposits from internal surfaces. If you switch from mineral-based oil to synthetic-based oil, these deposits will be removed by the esters in the synthetic oil and go into suspension. This suspended crud can clog oil filters, block oil passages and hence cause oil starvation. Before switching to synthetic oils on high-mileage engines (or if oil changes were far apart) when mineral oils were used, remove the valve cover and check for sludge deposits. If the engine is clean inside, go ahead. After switching, monitor the color of the oil on the dipstick and after around 1,000 miles remove the oil filter to see if it has an accumulation of visible deposits.

INTERMITTENT POWER LOSS

My 1999 Roadtrek motorhome with a Dodge 318 (5.2-liter) engine has an intermittent problem. After starting the engine cold and driving some distance, then shutting it off for anywhere from 15-60 minutes, I then start up and drive less than a mile and the engine starts to lose most power. I

then pull over to the side of the road and try to keep it running at low RPM for 30-60 seconds. During that time I hear a click and the engine returns to full power. The motorhome is fine for the rest of the day. No one can come up with an answer, or has heard of this from my description. They all say to leave it and they'll try to see if it happens, but it's very intermittent. I don't want to randomly throw in new parts and hope I'll find it. Any help would be appreciated.

MARTIN LEWIS | SPENCER, MASS.

The idle speed control solenoid could be faulty or may be getting a false signal to reduce idle speed. However, these are not noisy and shouldn't wipe out most power. An air conditioner clutch makes a loud click and the compressor could be dragging. Are you running the A/C or defroster? I suggest that you road test it with the engine cover removed to determine where the clicking sound originates. If any readers have encountered this problem, we'd like to hear from you.

CONTINUED ON PAGE 66

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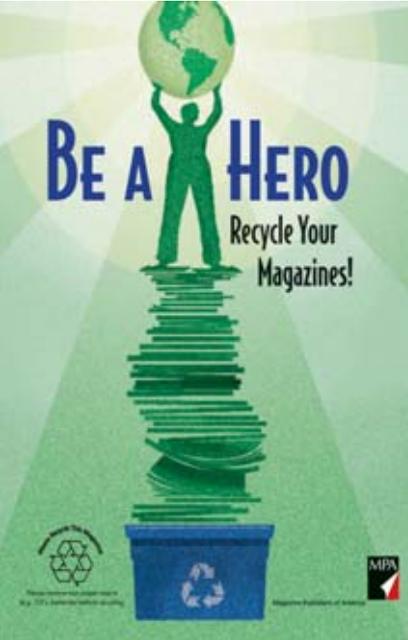
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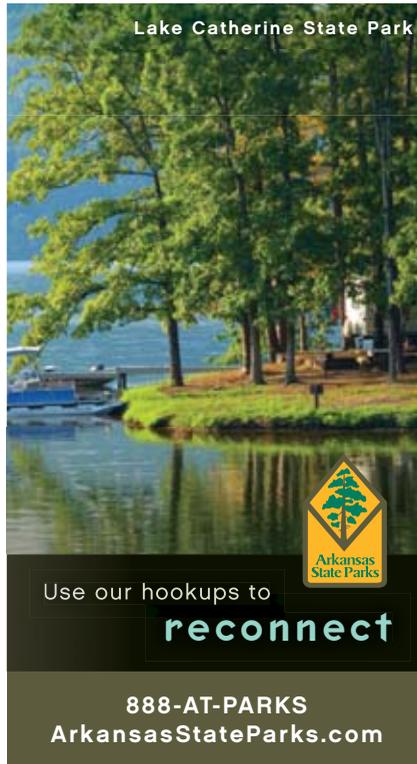
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SWAPPING REFRIGERATORS

I read an article that said ammonia absorption refrigerators were time bombs just waiting to explode. The author stated that he offered to retrofit any motorhome with a home-type refrigerator. This would also involve the addition of a suitable inverter, if necessary. What do you think? Is this a good idea?

ELDRIDGE THERIOT JR.
LAKE CHARLES, LA.

There are hundreds of thousands of absorption-type refrigerators safely and successfully in use. They are not time bombs. A home-type refrigerator with a compressor consumes a large amount of 120 volt AC power compared to an absorption type, which only uses 12 volt DC power for its electronics. If you plan to only use your refrigerator in an RV park with shorepower, you could convert to one. However, it's not very practical to use 12 volts from batteries run through an inverter to produce 120 volt AC power when parked without hookups. Also, absorption refrigerators have been around for decades and while they can leak, I've never heard of one exploding.

EVAPORATIVE RECOVERY SYSTEM

I have a problem with very high gas tank pressure. I have a 2006 Class A Fleetwood Bounder with a Ford V-10 engine. I realize there is a certain amount of pressure in a tank; however, a bang can be heard and felt when the tank expands. I recently made a trip in Florida and was running my generator using the A/C for cooling inside the

coach. The generator stopped running and I pulled over to check and found high enough pressure in the gas tank to shut off the generator. When I released the pressure the generator started. This happened several times, so I loosened the cap and was able to release the tank pressure. I don't believe the fuel cap is the issue, with the amount of pressure build up. Can you help?
BRAD TYLER | PHOENIX

The gas tank has an evaporative-recovery system and uses check valves to allow air to enter the tank as fuel is consumed, and for pressure to escape when the fuel and air expand as they get warmer. Usually when the genset quits because of a problem like this it's because of a partial vacuum (lower pressure), not excess pressure. Try changing the carbon canister. If that doesn't help, change the gas cap or take it to a chassis dealer for evaporative system testing.

BATTERY STORAGE DURING WINTER

I live in Canada. Typically I remove the two 6-volt batteries from the coach and store them in an unheated garage. I would like to keep the batteries charged during the winter period. Could you recommend a charger that will suit my needs? Also is it possible to charge both batteries at the same time?

DAVID BREWSTER
CALGARY, ALBERTA, CANADA

Yes, use a maintenance charger such as a 12-volt Battery Tender (available from

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DIESEL INJECTOR CLEANER

Fast Fuel Diesel Injector Cleaner by Prolong Super Lubricants is designed to improve fuel injector and intake valve performance, while increasing fuel economy. Prolong's Diesel Injector Cleaner is formulated to use in turbodiesel injection and turbodiesel common rail injection engines to remove gum, deposits and sludge build-up from fuel injectors, restoring a full spray pattern and allowing for quick starts. In addition, Prolong reports that its Diesel Injector Cleaner helps maintain diesel emission compliance and protects against corrosion and premature fuel injector and valve malfunction. It is compatible with all grades of diesel fuel and should be used when the vehicle starts misfiring, running rough or getting poor mileage. To use, simply pour the contents of the container into the fuel tank. One 12-ounce bottle treats up to 20 gallons of diesel fuel. For more information, call 800-540-5823 or visit www.prolong.com.



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BACK TO BASICS Towing a Dinghy Vehicle

For most motorhome owners, towing a vehicle — or dinghy as it is often called — is essential.

TOWING CHOICES

The essential ingredients for successful dinghy towing include a hitch receiver on the motorhome, a tow bar, an attachment bracket (baseplate) for the dinghy vehicle, safety cables, a taillight/brakelight wiring cable plus an supplemental braking system for the dinghy. Also, the motorhome's gross combination weight rating (GCWR) and the hitch receiver should be adequate for the weight of the dinghy.

Among tow bar choices, old-fashioned rigid A-frame-shaped bars are the least expensive, although not as convenient because they use a hitch ball and coupler. The preferred choice is a motorhome-mounted bar that utilizes a pivoting coupler inserted into a hitch receiver. Telescoping arms of these bars are extended to connection points on the dinghy vehicle. While not towing, the bars are retracted into the stored position on the motorhome.

A bracket or baseplate designed individually for each dinghy vehicle provides tow bar attachment points and distributes the towing load to the dinghy's strongest frame points.

The tow bar should be installed as level as possible; if not level, the front of the bar (attached to the motorhome) should be higher than the rear by not more than 3 inches.

These items, as well as a taillight/brakelight converter, are available from RV dealers and RV supply outlets across the country. Two primary towing equipment manufacturers, Roadmaster (www.roadmasterinc.com) and Blue Ox Products (www.blueox.us) — as well as other websites that can be accessed via an Internet search — offer consumers product information to help in making the best equipment choices.

Information on auxiliary braking systems for the dinghy vehicle is available from manufacturers specializing in these types of safety devices. With such systems, manufacturers claim up to a one-third reduction in stopping distances.

ON THE ROAD

Telescoping arms of an adjustable tow bar make hitching easy, allowing hookup even while the motorhome and dinghy vehicle are not parallel. Following a checklist that covers your situation while hitching/unhitching is essential, as is inspecting equipment regularly. Most tow bars are attached to brackets on dinghy vehicles with steel pins, held in place by wire clips; having all of them in place every time you tow is crucial.

GENERAL TOWING CHECKLIST

- Set emergency brake.
- Position tow bar and attach pins and clips.
- Attach safety cables and wiring harness.
- Check taillights/brakelights.
- Using a spare key, unlock steering wheel, position shift lever(s) according to manufacturer's instructions.
- Set auxiliary braking system.
- Release emergency brake.
- Have your travel companion check each item as well.
- Check visually to ensure wheels are rolling and front wheels are steering.
- Re-check pins, clips, cables and tires every time you stop.

Many motorhomes have rear overhangs as long as 12 feet, and tight turns exert considerable lateral force on the dinghy vehicle. The dinghy literally is dragged sideways in tight turns while accelerating from a stop. Therefore, it's always best to begin turning the steering wheel after the motorhome is moving, and to steer as widely as possible through turns. Backing usually is not possible, at least not more than a few feet before the front wheels of the dinghy turn to full lock in one direction or the other.

Dinghy towing can be as much fun as motorhoming itself, providing unlimited travel flexibility after the coach is parked. ♦



Top to bottom: A motorhome-mounted tow bar is the preferred choice for towing a dinghy vehicle. Baseplates, designed for specific vehicles, should be bolted to the frame. Removal of the front bumper and minor modifications may be necessary. Brackets for hookup to the tow bar are typically secured with steel pins and wire clips. Wiring connectors on the motorhome and dinghy vehicle should be inspected annually. One-way diodes are used in the dinghy wiring harness to prevent electrical feedback.



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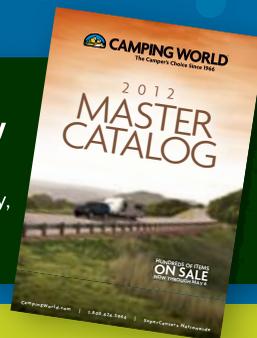
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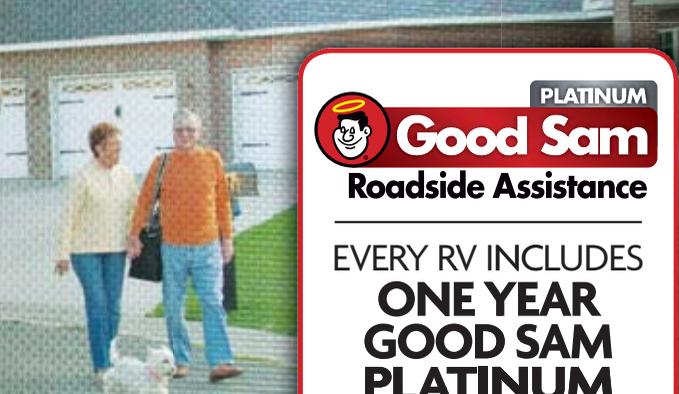
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USED 2004 Forest River GEORGETOWN 30 WAS \$43,892 **NOW \$29,995** SAVE \$13,897 (888) 674-0275 Gulf Breeze, FL STK# GB40289



USED 2003 R-Vision CONDOR 1281 WAS \$59,995 **NOW \$29,995** SAVE \$30,000 (888) 903-6787 Harrisburg, PA STK# HAR72373



USED 1999 Winnebago CHIEFTAIN 35C WAS \$49,995 **NOW \$29,995** SAVE \$20,000 (888) 682-3036 Churchville, NY STK# CHU69392



USED 2000 Fleetwood BOUNDER 34D WAS \$62,725 **NOW \$34,955** SAVE \$27,770 (888) 315-7967 Tucson, AZ STK# TUC18131



USED 2009 Winnebago CHALET 29T WAS \$49,815 **NOW \$34,978** SAVE \$14,837 (888) 859-6653 Hillsboro, OR STK# HIL37471



USED 2008 Itasca IMPULSE 29TR **NOW \$34,995** (888) 331-1595 Santa Clarita, CA STK# AMRV21883 VIN# 7DB42060



USED 2000 Pace Arrow VISION 36B **NOW \$34,995** (888) 668-6715 Katy, TX STK# KATC9410A



USED 2009 Winnebago CHALET 24VR WAS \$58,950 **NOW \$34,995** SAVE \$23,955 (888) 315-7967 Tucson, AZ STK# TUC14331



USED 2009 Winnebago CHALET 24V WAS \$58,530 **NOW \$34,995** SAVE \$23,535 (888) 315-7967 Tucson, AZ STK# TUC14367



USED 1999 Fleetwood PACE ARROW 37 VISION WAS \$54,999 **NOW \$35,895** SAVE \$19,104 (866) 502-8238 Bath, NY STK# BAT72202



USED 2004 Itasca SPIRIT 331T WAS \$54,995 **NOW \$36,995** SAVE \$18,000 (866) 754-7302 Greenwood, IN STK# GRW326464



USED 2009 Winnebago CHALET 29T WAS \$54,995 **NOW \$36,995** SAVE \$18,000 (866) 415-9726 Wheat Ridge, CO STK# KC511425



USED 2002 National RV DOLPHIN 5355 WH **NOW \$37,995** (888) 668-6715 Katy, TX STK# KATC9550A



USED 2000 Fleetwood BOUNDER 36U WAS \$49,995 **NOW \$38,995** SAVE \$11,000 (866) 775-0682 Roanoke, VA STK# ROA17027



USED 2004 Coachmen CONCORD 235SO
NOW \$39,875 (888) 641-0925 Kaysville,
UT STK# KY38486



USED 2003 Forest River WINDSONG 353DS WAS \$59,999
NOW \$39,898 SAVE \$20,101 (866) 648-5288 St. Augustine, FL
STK# JAC38936



USED 2010 Four Winds CHATEAU 28A
NOW \$39,900 (888) 844-1758 New Braunfels, TX STK# SW10037



USED 2000 Winnebago CHIEFTAN 34Y **NOW \$39,900** (888) 614-5201 Draper, UT STK# ABRV39382



USED 2010 Four Winds CHATEAU 21C
NOW \$39,943 (888) 480-3175 Sacramento, CA STK# SAC13506 VIN# DA58699



USED 2004 Itasca SUNOVA 35N WAS \$59,950 **NOW \$39,950** SAVE \$20,000 (888) 850-5575 Wood Village, OR STK# POR37746



USED 2003 Forest River LEXINGTON 270 GTS WAS \$68,290 **NOW \$39,959** SAVE \$28,331 (888) 484-3906 Spartanburg, SC STK# GR125427



USED 2001 Itasca SUNFLYER M-35U
NOW \$39,995 (888) 668-6715 Katy, TX STK# KATC9339D



USED 2010 Four Winds CHATEAU 31R WAS \$69,998 **NOW \$39,995** SAVE \$30,003 (888) 484-3906 Spartanburg, SC STK# GR125576



USED 2006 Coachmen FREELANDER 2600 SO WAS \$64,859 **NOW \$39,995** SAVE \$24,864 (888) 450-7060 Dothan, AL STK# DOT39812



USED 2008 Winnebago CHALET 30BR WAS \$69,900 **NOW \$39,999** SAVE \$29,901 (888) 489-5784 Knoxville, TN STK# KNOX19650



USED 2010 Four Winds CHATEAU 28A WAS \$54,888 **NOW \$41,500** SAVE \$13,388 (888) 568-2177 Burlington, WA STK# FRV10276



USED 2003 Fleetwood TIOGA 23E **NOW \$41,892** (888) 480-3175 Sacramento, CA STK# SAC15474A VIN# HB00867



USED 2004 Tiffin ALLEGRO 30DA **NOW \$41,995** (888) 479-3568 Vacaville, CA STK# VAC21120 VIN# 30A03915



USED 1999 Tiffin ALLEGRO BUS 37DSL
NOW \$42,995 (888) 668-6715 Katy, TX STK# KAT9307A



USED 2005 Coachmen LEPRECHAUN 307KS WAS \$64,245 **NOW \$43,977** SAVE \$20,268 (888) 454-1707 Kissimmee, FL STK# KS8114B



USED 2010 Four Winds CHATEAU 31P WAS \$72,450 **NOW \$44,805** SAVE \$27,645 (866) 820-0335 Council Bluffs, IA STK# CBI14805



USED 2001 Holiday Rambler ENDEAVOR 38PBD NOW \$44,900 (888) 844-1758 New Braunfels, TX STK# SW9741A



USED 2009 Winnebago CHALET 29T NOW \$44,995 (877) 834-0429 Bakersfield, CA STK# SLV22319 VIN# 8DB09338



USED 2008 Gulf Stream YELLOWSTONE 6237Y WAS \$64,900 **NOW \$46,777** SAVE \$18,123 (888) 489-5784 Knoxville, TN STK# KNOX19659



USED 1999 Winnebago CHIEFTAIN 36L WAS \$59,995 **NOW \$44,995** SAVE \$15,000 (888) 474-1330 Bartow, FL STK# DCW7848B



USED 2009 Damon DAYBREAK SPORT 3211 WAS \$62,995 **NOW \$45,995** SAVE \$17,000 (866) 415-9726 Wheat Ridge, CO STK# KC511791



USED 2008 Dutchmen DUTCHMEN 31F NOW \$46,978 (888) 641-0925 Kaysville, UT STK# KY37985



USED 2009 Winnebago CHALET 30B NOW \$47,995 (888) 479-3568 Vacaville, CA STK# VAC13158 VIN# 0A02623



USED 2007 Holiday Rambler ARISTA 310 WAS \$79,995 **NOW \$47,995** SAVE \$32,000 (866) 754-7302 Greenwood, IN STK# GRW326542



USED 2009 Winnebago CHALET 30BR NOW \$48,995 (888) 331-1595 Santa Clarita, CA STK# AMRV21880 VIN# 80A02611



USED 2001 Fleetwood EXPEDITION 36T NOW \$49,777 (888) 727-2952 North Little Rock, AR STK# LIT16863



USED 1998 Holiday Rambler IMPERIAL 40WDS WAS \$84,995 **NOW \$49,858** SAVE \$35,137 (866) 648-5288 St. Augustine, FL STK# JAC38679



USED 2000 Damon ESCAPER 3980 WAS \$69,950 **NOW \$49,963** SAVE \$19,987 (888) 850-5575 Wood Village, OR STK# POR38510



USED 2011 Coachmen CYNARA 3455 C WAS \$70,783 **NOW \$49,988** SAVE \$20,795 (888) 386-5187 Houghton Lake, MI STK# HL6205A



USED 2009 Gulf Stream YELLOWSTONE 6237 WAS \$68,790 **NOW \$49,988** SAVE \$18,802 (888) 386-5187 Houghton Lake, MI STK# HL5636A



USED 2010 Four Winds HURRICANE 31D NOW \$49,995 (888) 617-5406 Oakwood, GA STK# ORV16676



USED 2008 Winnebago CHALET 30BR NOW \$49,995 (877) 834-0429 Bakersfield, CA STK# SLV21887 VIN# 70A00559



USED 2001 Georgie Boy CRUISE MASTER 3816 DIESEL WAS \$73,995 **NOW \$49,995** SAVE \$24,000 (888) 682-3036 Churchville, NY STK# CHU70662



USED 2006 Winnebago VIEW 23 WAS \$68,888 **NOW \$52,995** SAVE \$15,893 (888) 568-2177 Burlington, WA STK# FRV10156



USED 2007 Four Winds HURRICANE 33-H WAS \$115,895 **NOW \$53,884** SAVE \$62,011 (866) 755-9604 Charleston, SC STK# CHN126845



USED 2009 Winnebago CHALET 30B **NOW \$53,981** (888) 480-3175 Sacramento, CA STK# SAC14088 VIN# 0A02636



USED 2009 Winnebago CHALET 30BR **NOW \$54,995** (877) 834-0429 Bakersfield, CA STK# SLV22353 VIN# 80A02634



USED 2010 Four Winds FREEDOM ELITE 26E **NOW \$54,995** (888) 668-6715 Katy, TX STK# KAT9614



USED 2004 Winnebago ADVENTURER 38R WAS \$80,790 **NOW \$54,995** SAVE \$25,795 (866) 610-5049 Akron, OH STK# SRV19382



NEW 2011 Four Winds FREEDOM ELITE 21C WAS \$73,794 **NOW \$54,995** SAVE \$18,799 (888) 692-9016 Myrtle Beach, SC STK# MB125102



USED 2005 National SEA BREEZE 8311 WORKHORSE WAS \$85,000 **NOW \$54,999** SAVE \$30,001 (866) 820-0335 Council Bluffs, IA STK# CBI16602



USED 2007 Winnebago VIEW 23H WAS \$78,599 **NOW \$54,999** SAVE \$23,600 (877) 554-3516 Lakewood, NJ STK# LAK8817



USED 1999 Gulf Stream TOURMASTER 8405 **NOW \$58,987** (888) 479-3568 Vacaville, CA STK# VAC18841 VIN# C031388



USED 2007 Winnebago VISTA 33T **NOW \$59,995** (888) 801-3226 Byron, GA STK# MAC17086



USED 2001 Fleetwood BOUNDER 39Z **NOW \$59,995** (888) 641-0925 Kaysville, UT STK# KY39366



USED 2000 Newmar DUTCHSTAR 3858 WAS \$89,995 **NOW \$59,995** SAVE \$30,000 (888) 315-7967 Tucson, AZ STK# TUC18116



NEW 2011 Four Winds FREEDOM ELITE 28U WAS \$89,657 **NOW \$59,995** SAVE \$29,662 (866) 754-7302 Greenwood, IN STK# GRW325876



USED 2008 Winnebago SIGHTSEER 30B WAS \$81,676 **NOW \$59,995** SAVE \$21,681 (866) 673-0250 Statesville, NC STK# STA125977



USED 2004 Coachmen CROSS COUNTRY 372DS WAS \$91,053 **NOW \$59,999** SAVE \$31,054 (877) 554-3516 Lakewood, NJ STK# LAK8895



USED 2008 Gulf Stream ULTRA 6319
 WAS \$79,998 **NOW \$59,999** SAVE
 \$19,999 (888) 672-5049 Syracuse, NY
 STK# SYU66720



NEW 2011 Four Winds FREEDOM ELITE
28U MSRP \$88,713 **NOW \$61,745** SAVE
 \$26,968 (888) 905-0490 Anthony, TX STK#
 LC15642



USED 2005 Itasca SUNCRUISER 37B WAS \$89,855
NOW \$64,777 SAVE \$25,078 (877) 289-0812 Las
 Vegas, NV STK# LVN36769B



NEW 2011 Four Winds FREEDOM ELITE
28U MSRP \$92,024 **NOW \$62,999** SAVE
 \$29,025 (877) 554-3516 Lakewood, NJ
 STK# LAK8580



USED 2008 Gulf Stream BT CRUISER
5272 **NOW \$63,850** (888) 815-7523
 Woodstock, GA STK# WDS16274



NEW 2011 Four Winds FREEDOM ELITE 31R MSRP
 \$91,408 **NOW \$64,872** SAVE \$26,536 (888) 905-
 0490 Anthony, TX STK# LC15714



NEW 2011 Four Winds FREEDOM ELITE
28U MSRP \$93,025 **NOW \$64,880** SAVE
 \$28,145 (888) 641-0925 Kaysville, UT
 STK# KY37157



NEW 2011 Four Winds FREEDOM ELITE
28U MSRP \$91,750 **NOW \$64,892** SAVE
 \$26,858 (888) 614-5201 Draper, UT STK#
 ABRV37156



NEW 2011 Four Winds
FREEDOM ELITE 28U
 WAS \$86,286 **NOW**
\$64,988 SAVE \$21,298
 (888) 386-5187 Houghton
 Lake, MI STK# HL5796



USED 2009 Forest River GEORGETOWN
GTX 3600 WAS \$119,877 **NOW \$64,995**
 SAVE \$54,882 (888) 860-8684 Strafford,
 MO STK# STR15316A



NEW 2011 Four Winds WINDSPORT
30Q WAS \$91,903 **NOW \$64,995** SAVE
 \$26,908 (888) 454-1707 Kissimmee, FL
 STK# KS8459



NEW 2011 Four Winds FREEDOM ELITE
28U WAS \$91,613 **NOW \$64,995** SAVE
 \$26,618 (888) 242-0293 Madison, WI
 STK# MAD15124



NEW 2011 Four Winds FREEDOM ELITE
28U WAS \$90,666 **NOW \$64,995** SAVE
 \$25,671 (888) 457-4801 Colfax, NC STK#
 CFX3634



USED 1999 Gulf Stream TOUR MASTER
8405 WAS \$109,900 **NOW \$64,999** SAVE
 \$44,901 (888) 489-5784 Knoxville, TN
 STK# KNOX19675



USED 2010 Fleet-
wood QUEST 24E WAS
 \$89,999 **NOW \$65,985**
 SAVE \$24,014 (866)
 648-5288 St. Augustine,
 FL STK# JAC39514



USED 2008 Winnebago VIEW 24H **NOW**
\$66,696 (888) 479-3568 Vacaville, CA
 STK# VAC15403A VIN# 9319700



NEW 2011 Four Winds CHATEAU CITATION 28BK WAS \$103,593 **NOW \$66,995** SAVE \$36,598 (866) 610-5049 Akron, OH STK# SRV18035



NEW 2011 Four Winds FREEDOM ELITE 31R WAS \$96,413 **NOW \$66,995** SAVE \$29,418 (888) 242-0293 Madison, WI STK# MAD15123



NEW 2011 Four Winds FOUR WINDS 25C MSRP \$95,139 **NOW \$66,995** SAVE \$28,144 (888) 542-3819 Mesa, AZ STK# MES12640



NEW 2011 Jayco GREYHAWK 31FS WAS \$99,775 **NOW \$69,980** SAVE \$29,795 (888) 896-2801 Albuquerque, NM STK# ARV15819



USED 2002 Beaver PATRIOT THUNDER 92408 WAS \$187,500 **NOW \$69,995** SAVE \$117,505 (888) 860-8684 Strafford, MO STK# STR12689



NEW 2010 Winnebago ERA 170XL WAS \$119,258 **NOW \$69,995** SAVE \$49,263 (888) 902-8565 Tallahassee, FL STK# TAL38560



USED 2000 Safari CONTINENTAL PANTHER 425 WAS \$109,995 **NOW \$69,995** SAVE \$40,000 (888) 474-1330 Bartow, FL STK# DCW8442B



NEW 2011 Four Winds FREEDOM ELITE 23S WAS \$104,970 **NOW \$69,995** SAVE \$34,975 (888) 454-1707 Kissimmee, FL STK# KS8201



NEW 2011 Four Winds FOUR WINDS SIESTA 26BE WAS \$102,715 **NOW \$69,995** SAVE \$32,720 (888) 625-5187 Fort Myers, FL STK# FTM12433



NEW 2011 Itasca IMPULSE SILVER 260P MSRP \$94,601 **NOW \$69,995** SAVE \$24,606 (888) 905-0490 Anthony, TX STK# LC15674



NEW 2011 Four Winds FREEDOM ELITE 28U WAS \$94,255 **NOW \$69,995** SAVE \$24,260 (888) 674-0275 Gulf Breeze, FL STK# GB40022



USED 2007 Forest River GEORGETOWN 370TS WAS \$89,999 **NOW \$69,995** SAVE \$20,004 (866) 502-8238 Bath, NY STK# BAT70988



USED 2010 Coachmen FREELANDER 2100CB WAS \$89,995 **NOW \$69,995** SAVE \$20,000 (888) 902-8565 Tallahassee, FL STK# TAL40217



USED 2008 Gulf Stream BT CRUISER 5291 WAS \$89,888 **NOW \$69,995** SAVE \$19,893 (888) 672-5049 Syracuse, NY STK# SYU68548



USED 2006 Holiday Rambler VACATIONER 34PDD WAS \$100,853 **NOW \$69,999** SAVE \$30,854 (877) 554-3516 Lakewood, NJ STK# LAK8899



NEW 2011 Jayco GREYHAWK 31DS MSRP \$96,963 **NOW \$71,995** SAVE \$24,968 (888) 315-7967 Tucson, AZ STK# TUC16147



NEW 2011 Four Winds FREEDOM ELITE 26BE MSRP \$98,624 **NOW \$72,900** SAVE \$25,724 (888) 844-1758 New Braunfels, TX STK# SW10009



NEW 2011 Jayco GREYHAWK 31FK WAS \$103,440 **NOW \$73,988** SAVE \$29,452
(888) 896-2801 Albuquerque, NM STK# ARV15816



USED 2004 Coachmen CROSSCOUNTRY 354MBS WAS \$99,665 **NOW \$74,900**
SAVE \$24,765 (866) 673-0250 Statesville, NC STK# STA126315



USED 2001 Mountain High Coachworks SUMMITT 3990 WAS \$98,649 **NOW \$74,997** SAVE \$23,652
(877) 450-7415 Island Lake, IL STK# CRV5997A



USED 2002 Itasca HORIZON 36LD **NOW \$74,990** (888) 641-0925 Kaysville, UT
STK# KY39872



NEW 2011 Itasca IMPULSE SILVER 31CP
MSRP \$98,873 **NOW \$74,995** SAVE \$23,878 (888) 905-0490 Anthony, TX STK# LC15673



USED 2009 Coachmen FREEDOM VISION 3540DS
NOW \$75,988 (888) 801-3226 Byron, GA STK# MAC16618



USED 2008 Coachmen CONCORD 275DS
WAS \$125,881 **NOW \$75,995** SAVE \$49,886 (866) 755-9604 Charleston, SC
STK# CHN124687



NEW 2011 Jayco GREYHAWK 31FS WAS \$100,445 **NOW \$76,995** SAVE \$23,450
(888) 463-3995 Colorado Springs, CO STK# KC211858



NEW 2011 Four Winds HURRICANE 31D WAS \$104,811 **NOW \$77,950**
SAVE \$26,861 (888) 859-6653 Hillsboro, OR STK# HIL36319



USED 2007 Dynamax ISATA 254SL **NOW \$79,858** (888) 696-2165 Savannah, GA STK# SAV16409



NEW 2011 Holiday Rambler VACATION-ER 30SFS WAS \$115,852 **NOW \$79,995**
SAVE \$35,857 (866) 610-5049 Akron, OH STK# SRV18592



NEW 2011 Four Winds FOUR WINDS SIESTA 24SB MSRP \$111,289 **NOW \$79,995** SAVE \$31,294 (888) 542-3819
Mesa, AZ STK# MES16169



NEW 2011 Four Winds HURRICANE 32D WAS \$110,557 **NOW \$79,995** SAVE \$30,562 (888) 475-5918 Robertsdale, AL
STK# RD39404



USED 2011 Coachmen LEPRECHAUN 311 WAS \$105,000 **NOW \$79,995** SAVE \$25,005 (866) 755-9604 Charleston, SC
STK# CHN126216



USED 2007 Coachmen PATHFINDER 377DS
NOW \$85,995 (888) 617-5406 Oakwood, GA
STK# ORV16417



NEW 2011 Four Winds WINDSPORT 32V WAS \$129,734 **NOW \$88,888** SAVE \$40,846 (866) 820-0335 Council Bluffs, IA
STK# CBI11291



USED 2005 Newmar MOUNTAIN AIRE 3505 WAS \$129,850 **NOW \$89,877** SAVE \$39,973 (866) 996-3140 Calera, AL STK# CLE18967



USED 2004 Winnebago VECTRA 40QD WAS \$129,895 **NOW \$89,985** SAVE \$39,910 (888) 692-9016 Myrtle Beach, SC STK# MB124731



NEW 2011 Four Winds WINDSPORT 32V WAS \$141,266 **NOW \$89,995** SAVE \$51,271 (888) 457-4801 Colfax, NC STK# CFX4084



USED 2001 Newmar MOUNTAIN AIRE 40 WAS \$129,995 **NOW \$89,995** SAVE \$40,000 (866) 775-0682 Roanoke, VA STK# ROA18332



NEW 2011 Four Winds SERRANO 31Z LIST PRICE \$157,005 **NOW \$95,450** SAVE \$61,555 (888) 331-1595 Santa Clarita, CA STK# AMRV21125 VIN# A3440281



NEW 2011 Itasca REYO 25Q MSRP \$138,827 **NOW \$95,983** SAVE \$42,844 (888) 614-5201 Draper, UT STK# ABRV36387



NEW 2011 Itasca REYO 25Q LIST PRICE \$149,190 **NOW \$97,800** SAVE \$51,390 (888) 331-1595 Santa Clarita, CA STK# AMRV22456 VIN# A9453033



USED 2006 Newmar KOUNTRY STAR 3510 **NOW \$99,900** (888) 844-1758 New Braunfels, TX STK# SW-C9687C



USED 2008 Damon ASTORIA 3770 WAS \$139,995 **NOW \$119,995** SAVE \$20,000 (888) 242-0293 Madison, WI STK# MA-D17162A



USED 2006 Fleetwood PROVIDENCE 39L **NOW \$99,995** (888) 815-7523 Woodstock, GA STK# WDS16554



USED 2005 Itasca MERIDIAN 39K WAS \$127,988 **NOW \$99,995** SAVE \$27,993 (888) 475-5918 Robertsdale, AL STK# RD40283



USED 2006 Alfa GOLD 40FD WAS \$164,900 **NOW \$99,999** SAVE \$64,901 (866) 289-0113 Chattanooga, TN STK# CH7099



USED 2007 Fleetwood EXCURSION 40X WAS \$159,900 **NOW \$104,999** SAVE \$54,901 (866) 289-0113 Chattanooga, TN STK# CH7346



NEW 2011 Winnebago VIA 25R MSRP \$139,518 **NOW \$105,995** SAVE \$33,523 (888) 696-2165 Savannah, GA STK# SAV14765



NEW 2011 Itasca REYO 25Q LIST PRICE \$165,288 **NOW \$107,224** SAVE \$58,064 (888) 479-3568 Vacaville, CA STK# VAC12728 VIN# 9454891



NEW 2011 Winnebago VIA 25Q MSRP \$142,956 **NOW \$107,860** SAVE \$35,096 (888) 696-2165 Savannah, GA STK# SAV15877



NEW 2011 Four Winds SERRANO 31Z MSRP \$149,141 **NOW \$109,888** SAVE \$39,253 (888) 348-5909 Avondale, AZ STK# AV010470



NEW 2011 Itasca REYO 25T WAS \$140,037 **NOW \$109,989** SAVE \$30,048 (888) 672-5049 Syracuse, NY STK# SYU66936



USED 2006 Fleetwood DISCOVERY 39S WAS \$168,896 **NOW \$109,995** SAVE \$58,901 (888) 450-7060 Dothan, AL STK# DOT39685



NEW 2010 Itasca REYO 25R WAS \$146,980 **NOW \$109,998** SAVE \$36,982 (888) 457-4801 Colfax, NC STK# CFX3751



USED 2004 Itasca HORIZON 40AD 350HP WAS \$147,855 **NOW \$109,995** SAVE \$37,860 (877) 289-0812 Las Vegas, NV STK# LVN37139A



NEW 2011 Itasca REYO 25Q MSRP \$141,678 **NOW \$109,995** SAVE \$31,683 (888) 617-5406 Oakwood, GA STK# ORV15969



USED 2006 Itasca MERIDIAN 32T **NOW \$113,744** (888) 480-3175 Sacramento, CA STK# SAC19008 VIN# CW69489



NEW 2011 Four Winds SERRANO 33A WAS \$163,457 **NOW \$114,610** SAVE \$48,847 (888) 682-3036 Churchville, NY STK# CHU70425



USED 2006 Gulf Stream FRIENDSHIP 8708 4/SLIDE WAS \$173,595 **NOW \$114,999** SAVE \$58,596 (866) 289-0113 Chattanooga, TN STK# CH6832



USED 2008 Winnebago DESTINATION 39W WAS \$159,995 **NOW \$119,995** SAVE \$40,000 (888) 568-2177 Burlington, WA STK# FRV10268



USED 2007 Gulf Stream TOUR MASTER T40C WAS \$169,900 **NOW \$123,670** SAVE \$46,230 (888) 489-5784 Knoxville, TN STK# KNOX19766



USED 2007 Fleetwood PROVIDENCE 39L **NOW \$127,889** (888) 801-3226 Byron, GA STK# MAC15677



USED 2007 Coachmen SPORTSCOACH LEGEND 40QS **NOW \$129,950** (888) 480-3175 Sacramento, CA STK# SAC16290 VIN# CX12364



USED 2008 Winnebago DESTINATION 37G WAS \$169,988 **NOW \$129,988** SAVE \$40,000 (888) 475-5918 Robertsdale, AL STK# RD40443



USED 2007 Gulf Stream FRIENDSHIP G8 8412 **NOW \$129,995** (888) 815-7523 Woodstock, GA STK# WDS16168



USED 2007 Damon TUSCANY 4072 WAS \$199,995 **NOW \$129,995** SAVE \$70,000 (888) 450-7060 Dothan, AL STK# DOT39994



USED 2006 Holiday Rambler SCEPTER 40PDQ WAS \$177,870 **NOW \$129,995** SAVE \$47,875 (888) 896-2801 Albuquerque, NM STK# ARV15579



USED 2003 Newmar DUTCHSTAR 4050
 WAS \$170,800 **NOW \$129,995** SAVE
 \$40,805 (888) 454-1707 Kissimmee, FL
 STK# KSC2178



**USED 2007 Mo-
 naco KNIGHT 40DFT**
 WAS \$169,988 **NOW**
\$129,995 SAVE
 \$39,993 (888) 475-
 5918 Robertsdale, AL
 STK# RD40236



USED 2008 Gulf Stream CRESCENDO
40UL WAS \$184,900 **NOW \$139,516**
 SAVE \$45,384 (866) 289-0113 Chatta-
 nooga, TN STK# CH6815



USED 2006 Travel Supreme TRAVEL SUPREME
40DS04 WAS \$179,995 **NOW \$139,995** SAVE
 \$40,000 (877) 774-0328 Ocala, FL STK# OCA20782



USED 2006 Winnebago HORIZON 40FD
NOW \$149,995 (888) 815-7523 Wood-
 stock, GA STK# WDS16107



USED 2009 Safari SAFARI CHEETAH
42PAQ WAS \$230,990 **NOW \$149,995**
 SAVE \$80,995 (866) 610-5049 Akron, OH
 STK# SRV19512



NEW 2011 Damon
ASTORIA 40KT WAS
 \$248,128 **NOW**
\$167,841 SAVE \$80,287
 (877) 450-7415 Island
 Lake, IL STK# CRV5954



USED 2008 Holiday
Rambler ENDEAVOR
40SKQ WAS \$229,781
NOW \$168,488 SAVE
 \$61,293 (866) 755-9604
 Charleston, SC STK#
 CHN127823



NEW 2011 Damon ASTORIA 40KT MSRP
 \$259,982 **NOW \$169,859** SAVE \$90,123
 (888) 905-0490 Anthony, TX STK# LC15757



NEW 2011 Damon TUSCANY 4078
 WAS \$249,627 **NOW \$169,867** SAVE
 \$79,760 (888) 450-7060 Dothan, AL STK#
 DOT40837



NEW 2011 Damon ASTORIA 40KT WAS
 \$253,597 **NOW \$169,995** SAVE \$83,602
 (888) 242-0293 Madison, WI STK#
 MAD16662



NEW 2011 Damon
ASTORIA 40KT WAS
 \$253,165 **NOW**
\$169,995 SAVE
 \$83,170 (866) 754-7302
 Greenwood, IN STK#
 GRW326201



NEW 2011 Itasca MERIDIAN V CLASS
34Y MSRP \$231,771 **NOW \$169,995**
 SAVE \$61,776 (888) 617-5406 Oakwood,
 GA STK# ORV15855



NEW 2011 Damon ASTORIA 40KT WAS
 \$256,101 **NOW \$169,999** SAVE \$86,102
 (877) 554-3516 Lakewood, NJ STK#
 LAK8889



NEW 2011 Damon TUSCANY 4051 WAS
 \$251,859 **NOW \$174,999** SAVE \$76,860
 (866) 415-9726 Wheat Ridge, CO STK#
 KC511239



NEW 2011 Damon TUSCANY 4078 WAS
 \$252,016 **NOW \$178,612** SAVE \$73,404
 (888) 848-7312 Columbia, SC STK#
 COL16971



NEW 2011 Damon ASTORIA 40KT WAS
 \$248,728 **NOW \$178,879** SAVE \$69,849
 (888) 672-5049 Syracuse, NY STK#
 SYU67794



NEW 2011 Damon ASTORIA 40KT WAS \$247,769 **NOW \$179,892** SAVE \$67,877 (888) 903-6787 Harrisburg, PA STK# HAR70426



NEW 2011 Damon ASTORIA 40BQ WAS \$243,600 **NOW \$179,995** SAVE \$63,605 (888) 450-7060 Dothan, AL STK# DOT40836



USED 2006 Beaver PATRIOT THUNDER VICKSBURG 525 QUAD WAS \$293,849 **NOW \$189,995** SAVE \$103,854 (888) 542-3819 Mesa, AZ STK# MES19890



NEW 2011 Winnebago JOURNEY 40L WAS \$268,281 **NOW \$179,999** SAVE \$88,282 (866) 289-0113 Chattanooga, TN STK# CH6363



NEW 2011 Damon TUSCANY 4051 WAS \$258,982 **NOW \$182,998** SAVE \$75,984 (888) 457-4801 Coffax, NC STK# CFX4150



NEW 2011 Itasca MERIDIAN V CLASS 39N WAS \$246,787 **NOW \$189,995** SAVE \$56,792 (866) 996-3140 Calera, AL STK# CLE18939



NEW 2011 Itasca MERIDIAN 40U LIST PRICE \$286,735 **NOW \$196,220** SAVE \$90,515 (888) 331-1595 Santa Clarita, CA STK# AMRV21739 VIN# BCAY7953



NEW 2011 Damon TUSCANY 42FK WAS \$315,937 **NOW \$212,995** SAVE \$102,942 (866) 415-9726 Wheat Ridge, CO STK# KC511531



NEW 2011 Damon TUSCANY 42RQ WAS \$307,453 **NOW \$212,995** SAVE \$94,458 (888) 819-7952 Longmont, CO STK# KC111333



NEW 2011 Winnebago JOURNEY 40L WAS \$299,059 **NOW \$215,580** SAVE \$83,479 (866) 775-0682 Roanoke, VA STK# ROA13987



NEW 2011 Damon TUSCANY 42FK WAS \$313,863 **NOW \$219,850** SAVE \$94,013 (866) 775-0682 Roanoke, VA STK# ROA16310



NEW 2011 Winnebago JOURNEY 40L WAS \$280,333 **NOW \$219,995** SAVE \$60,338 (888) 727-2952 North Little Rock, AR STK# LIT14119



NEW 2011 Itasca MERIDIAN 40U WAS \$285,053 **NOW \$219,999** SAVE \$65,054 (888) 471-3546 Chichester, NH STK# GRV507245



NEW 2011 Damon TUSCANY 42FK WAS \$317,818 **NOW \$224,995** SAVE \$92,823 (877) 289-0812 Las Vegas, NV STK# LVN38227



NEW 2011 Damon TUSCANY 42RQ WAS \$307,759 **NOW \$224,995** SAVE \$82,764 (877) 289-0812 Las Vegas, NV STK# LVN38784



NEW 2011 Itasca ELLIPSE 40CD WAS \$310,626 **NOW \$239,998** SAVE \$70,628 (888) 471-3546 Chichester, NH STK# GRV508007

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USED CLASS B & C MOTORHOMES

YEAR/MANUFACTURER/MAKE/MODEL	STOCK #	LIST	NOW*	PHONE NO.
1975 Winnebago MINNIE DODGE	MER19691	\$6,995	\$5,858	(888) 254-9145
2003 Shasta REVERE 242	BAT72544	\$28,999	\$23,995	(866) 502-8238
2004 Fleetwood JAMBOREE 26Q	POR39145	\$34,950	\$29,988	(888) 850-5575
2010 Four Winds CHATEAU 28A	KC111795	\$45,990	\$38,995	(888) 819-7952
2010 Four Winds CHATEAU 28A	KC111797	\$42,990	\$39,995	(888) 819-7952
2010 Four Winds CHATEAU 28A	KC111796	\$44,990	\$42,995	(888) 819-7952
2005 Coachmen CONCORD 235 SO	KC210685B	\$54,997	\$45,997	(888) 463-3995
2003 Coachmen SANTARA 31	GB40685C	\$55,300	\$48,995	(888) 674-0275
2003 Four Winds FREEDOM ELITE 21C	BR126808	\$67,500	\$54,995	(888) 488-3906
2007 Forest River LEXINGTON 283 GTS	HAR72575	\$61,995	\$54,995	(888) 903-6787
2007 Forest River LEXINGTON 300SS	POR37422	\$74,950	\$59,950	(888) 850-5575
2009 Coachmen FREELANDER 2700RS	CHU72720	\$69,995	\$59,995	(888) 682-3036
2009 Gulf Stream BT CRUISER 5272 GX2	KC110788B	\$69,990	\$59,995	(888) 819-7952
2007 Jayco GREYHAWK 30GS	KC2CS8923B	\$69,997	\$59,997	(888) 463-3995
2009 Four Winds SIESTA 31BH	OCA19149	\$69,995	\$62,888	(877) 774-0328
2010 Four Winds CHATEAU 31B	GRV507549	\$68,955	\$64,999	(888) 471-3546
2008 Winnebago VIEW 24J	HIL39487	\$82,000	\$74,600	(888) 859-6653
2009 Gulf Stream VISTA CRUISER 4232-MINI	KC211041B	\$94,997	\$79,997	(888) 463-3995

NEW CLASS B & C MOTORHOMES

YEAR/MANUFACTURER/MAKE/MODEL	STOCK #	LIST	NOW*	PHONE NO.
2011 Four Winds FREEDOM ELITE 21C	LVN40261	\$73,346	\$59,995	(877) 289-0812
2011 Four Winds FREEDOM ELITE 21C	CFX4269	\$72,891	\$70,889	(888) 457-4801
2011 Winnebago ACCESS 26Q	MAC16867	\$90,224	\$75,900	(888) 801-3226
2011 Jayco GREYHAWK 31SS	ABRV37274	\$94,616	\$77,897	(888) 614-5201
2012 Jayco GREYHAWK 31SS	ARV16105	\$95,670	\$78,980	(888) 896-2801
2011 Winnebago ACCESS 31N	SLV21219	\$99,636	\$79,381	(877) 834-0429
2011 Four Winds FREEDOM ELITE 23S	COL15148	\$97,691	\$79,995	(888) 848-7312
2011 Itasca IMPULSE 26Q	GRV507420	\$90,325	\$79,999	(888) 471-3546
2011 Four Winds FREEDOM ELITE 23S	LIT13830	\$103,498	\$84,995	(888) 727-2952

USED CLASS A GAS MOTORHOMES

YEAR/MANUFACTURER/MAKE/MODEL	STOCK #	LIST	NOW*	PHONE NO.
1981 Forest River SANDPIPER CLASS A	HL5968B	\$7,995	\$6,988	(888) 386-5187
1987 Fleetwood SOUTHWIND 31LB	CRV5796A	\$9,995	\$7,995	(877) 450-7415
1989 Georgie Boy CRUISE MASTER 28	MER19497	\$10,995	\$8,858	(888) 254-9145
1994 Itasca SUNRISE 32	FRV10801	\$13,995	\$12,995	(888) 568-2177
1990 Fleetwood FLAIR 22	MER14794A	\$19,995	\$15,858	(888) 254-9145
1994 Itasca SUNOVA 315	BAT70565	\$24,995	\$19,995	(866) 502-8238
1997 Fleetwood PACE ARROW 37	STA126619	\$29,995	\$25,557	(866) 673-0250
2005 Fleetwood TERRA 31H	OCA20820	\$34,995	\$29,888	(877) 774-0328
2003 Winnebago SIGHTSEER 31B	GR126801	\$39,995	\$33,995	(888) 484-3906
2001 Fleetwood SOUTHWIND 32V	FTM1101D	\$39,995	\$34,995	(888) 625-5187
2000 Fleetwood PACE ARROW VISION 36Z	TAL40571	\$46,995	\$37,995	(888) 902-8565
2000 Fleetwood PACE ARROW VISION 36Z	JAX19972	\$47,429	\$39,639	(877) 458-3214
2006 Four Winds HURRICANE 31D	GB41597	\$45,670	\$42,995	(888) 674-0275
2008 Winnebago CHALET 30BR	POR36785	\$56,814	\$44,950	(888) 850-5575
2004 Damon INTRUDER 375F	JAX19976	\$59,425	\$51,795	(877) 458-3214
2003 Four Winds WINDSPORT 33SL	JAX19974	\$59,645	\$52,669	(877) 458-3214
2006 Damon CHALLENGER 348F	ARV16031P	\$64,750	\$56,900	(888) 896-2801
2010 Four Winds HURRICANE 30Q	JAX20217	\$69,669	\$59,999	(877) 458-3214
2006 Monaco MONARCH 36PBD	AV015225	\$79,888	\$69,888	(888) 348-5909
2009 Damon DAYBREAK SPORT 3204	AV014351	\$79,888	\$69,888	(888) 348-5909
2007 Georgie Boy CRUISEMASTER 3580TS	KNOX20299	\$84,900	\$72,891	(888) 489-5784
2011 Damon DAYBREAK 35BD	HAR72857	\$86,995	\$79,995	(888) 903-6787
2006 Rexhall REKXAIR 340GT	AV014356	\$89,888	\$84,888	(888) 348-5909
2008 Winnebago ADVENTURER 38J	CHN126309	\$112,988	\$99,987	(866) 755-9604

NEW CLASS A GAS MOTORHOMES

YEAR/MANUFACTURER/MAKE/MODEL	STOCK #	LIST	NOW*	PHONE NO.
2011 Itasca SUNSTAR 26P	ABRV37590	\$104,378	\$84,572	(888) 614-5201
2011 Itasca SUNSTAR 26P	LIT39153	\$100,867	\$84,995	(888) 902-8565
2011 Winnebago VISTA 26P	LIT14140	\$99,717	\$89,777	(888) 727-2952
2010 Damon DAYBREAK 34SS	COL18597	\$101,115	\$89,995	(888) 848-7312
2011 Itasca SUNCRUISER 35P	CLE18941	\$167,920	\$144,995	(866) 996-3140

USED CLASS A DIESEL MOTORHOMES

YEAR/MANUFACTURER/MAKE/MODEL	STOCK #	LIST	NOW*	PHONE NO.
1998 Holiday Rambler ENDEAVOR 37WD	FTM1257D	\$28,759	\$25,995	(888) 625-5187
1997 Fleetwood AMERICAN DREAM 39DAF	GB40228C	\$53,988	\$42,995	(888) 674-0275
2000 Itasca HORIZON 36LD	FTM11977A	\$62,254	\$49,995	(888) 625-5187
2000 Itasca HORIZON 36	HL6163A	\$60,790	\$55,988	(888) 386-5187
2001 Damon ULTRASPORT 3679	MB126222	\$72,835	\$59,995	(888) 692-9016
2000 Country Coach ALLURE 36SLD	STR16906B	\$70,000	\$59,995	(888) 860-8684
2000 Newmar DUTCH STAR 3858	FTM1248C	\$61,748	\$59,995	(888) 625-5187
1999 Fleetwood DISCOVERY 37V	MB126657	\$72,851	\$59,997	(888) 692-9016
1996 Country Coach INTRIGUE 36	FRV10200	\$69,995	\$59,999	(888) 568-2177
2004 Alfa SEE YA 36FD	STR17909A	\$84,650	\$67,995	(888) 860-8684
2000 Winnebago ULTIMATE ADVANTAGE 36C	MER20552	\$81,995	\$76,858	(888) 254-9145
2003 Monaco DIPLOMAT 40PBD	MER20208	\$84,995	\$78,858	(888) 254-9145
2003 Winnebago JOURNEY DL 39WD	TAL40278	\$89,995	\$79,995	(888) 902-8565
2002 National RV TRADE WINDS 7390	DCW8905	\$89,995	\$79,995	(888) 474-1330
2000 Monaco EXECUTIVE 40	AV015719	\$98,988	\$79,998	(888) 348-5909
2000 Gulf Stream TOURMASTER 8405	DCWC2198	\$89,995	\$84,995	(888) 474-1330
2006 Damon ASTORIA PACIFIC 3579	CBH18317	\$96,500	\$86,450	(866) 820-0335
2002 Newmar DUTCHSTAR 3892	JAC39949	\$104,995	\$86,900	(866) 648-5288
2005 Fleetwood EXPEDITION 38N	MAC17010	\$109,900	\$98,900	(888) 801-3226
2004 Tiffin ALLEGRO 38TGP	COL18539	\$131,798	\$105,858	(888) 848-7312
2007 Damon ASTORIA 3773	JAX19977	\$126,439	\$109,899	(877) 458-3214
2008 Holiday Rambler VACATIONER 34SBD	STA125968	\$139,995	\$114,985	(866) 673-0250
2007 Damon ASTORIA PACIFICA 3774	CBH16301	\$134,995	\$114,995	(866) 820-0335
2009 Monaco LA PALMA 36SFD	HIL38704	\$129,000	\$115,950	(888) 859-6653
2006 Holiday Rambler AMBASSADOR 40PLQ	OCA20819	\$139,995	\$124,888	(877) 774-0328
2008 Itasca LATITUDE 39W	KSC2190	\$146,623	\$129,995	(888) 454-1707
2001 Beaver Motor Coaches MARQUIS 40	HL40019	\$159,950	\$139,950	(888) 859-6653
2009 Gulf Stream CRESCENDO 8386	RD40508	\$169,988	\$139,988	(888) 475-5918
2007 Newmar KOUNTRY STAR 3912	OCA20809	\$169,995	\$147,888	(877) 774-0328
2007 Monaco DIPLOMAT 40SFT	MES16388	\$168,420	\$149,995	(888) 542-3819
2010 Four Winds MONTECITO 40J	CLE18956	\$199,000	\$157,250	(866) 996-3140
2007 Fleetwood REVOLUTION 40V	JAC38934	\$199,999	\$157,781	(866) 648-5288
2003 American EAGLE 42R	ROA17220	\$199,995	\$159,995	(866) 775-0682
2007 Itasca ELLIPSE 40TD	CHU72824	\$199,995	\$159,995	(888) 682-3036
2007 Fleetwood REVOLUTION 40E	HAR73239	\$174,995	\$159,995	(888) 903-6787
2007 Monaco CAMELOT 42PDQ	MES16518	\$184,850	\$169,995	(888) 542-3819
2010 Winnebago TOUR 40	STA126511	\$249,998	\$211,899	(866) 673-0250

NEW CLASS A DIESEL MOTORHOMES

YEAR/MANUFACTURER/MAKE/MODEL	STOCK #	LIST	NOW*	PHONE NO.
2011 Itasca REYO 25Q	GRV507290	\$128,465	\$102,999	(888) 471-3546
2011 Itasca REYO 25T	SW9726	\$134,206	\$106,500	(888) 844-1758
2011 Winnebago VIA 25Q	CLE18943	\$140,821	\$114,995	(866) 996-3140
2011 Itasca REYO 25Q	SLV21736	\$138,482	\$115,995	(877) 834-0429
2011 Four Winds SERRANO 33A	SYU73661	\$152,761	\$129,898	(888) 672-5049
2011 Winnebago JOURNEY 40U	SAV16619	\$282,612	\$229,960	(888) 696-2165

*Government fees, state taxes, title, license and dealer fees as applicable will be added to comply with state vehicle codes. Sales tax not applicable in OR. In Arizona, prices plus doc. fee of \$399 and pre-delivery inspection. In California, prices not inclusive of government fees and prep charge, finance charge, or emissions testing charge. Freight costs vary by state. Not applicable in CA, OH, AR, TX or OR. In AZ, CA, TX and AR, 'List Price' is equal to 'MSRP'. Inventory and floorplans vary by location. VIN numbers posted at dealership. Advertised inventory available at time of printing. Void where prohibited by law. Offer valid at any Camping World RV Sales or FreedomRoads dealer only. Dealer not responsible for typographical errors. See dealer for details. Offers expire 4/30/12.

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E-mail: sseidlitz@goodsamfamily.com **Fax:** 530-653-2090 Attn: Sue Seidlitz

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Camping World and other RV stores) and charge them in series. This will require that you use a jumper wire running from one positive (+) terminal to the other battery's negative (-) terminal. Then connect the other terminals to the battery charger.

MORE DEAD BATTERIES

The batteries in my 2006 Pace Arrow motorhome keep running down. One day while camping the roof A/C only worked intermittently. When it stopped, I started the engine and the air came on so I let the motor run for about 30 minutes. The A/C ran for much of the day, but then it cut off. I asked a man that worked on RVs to check it out and he said the batteries were dead, so I got new ones and all was well.

At the next campground I had the same trouble, the air wouldn't run and this time the motorhome wouldn't start and neither would the generator. I thought that when plugged in to the shorepower pedestal the converter would charge the batteries for the coach, therefore keeping the air on. I have battery cut-offs on my panel and once when I cut them off, it did

me no good because then I had no power to the coach and no air. What should I do?
ALAN SUMRALL | VIA EMAIL

Starting the engine has no effect on the roof air since it operates on 120-volt AC power. The engine-starting battery does not get charged when the engine is off and the coach is on shorepower. You need something like Triki-Start (www.islproducts.com/TLSPage.html) for that battery. However, the two coach batteries should be getting charged from the converter. To determine if it is working, take a voltage reading at each battery with the coach unplugged at rest, and then with it plugged in. Most converters overcharge the batteries when left on for long periods, which is why they use up so much water. When they run dry they get ruined. You should consider getting a "smart" converter such as a Xantrex Truecharge, which does a much better job of keeping batteries alive. ♦

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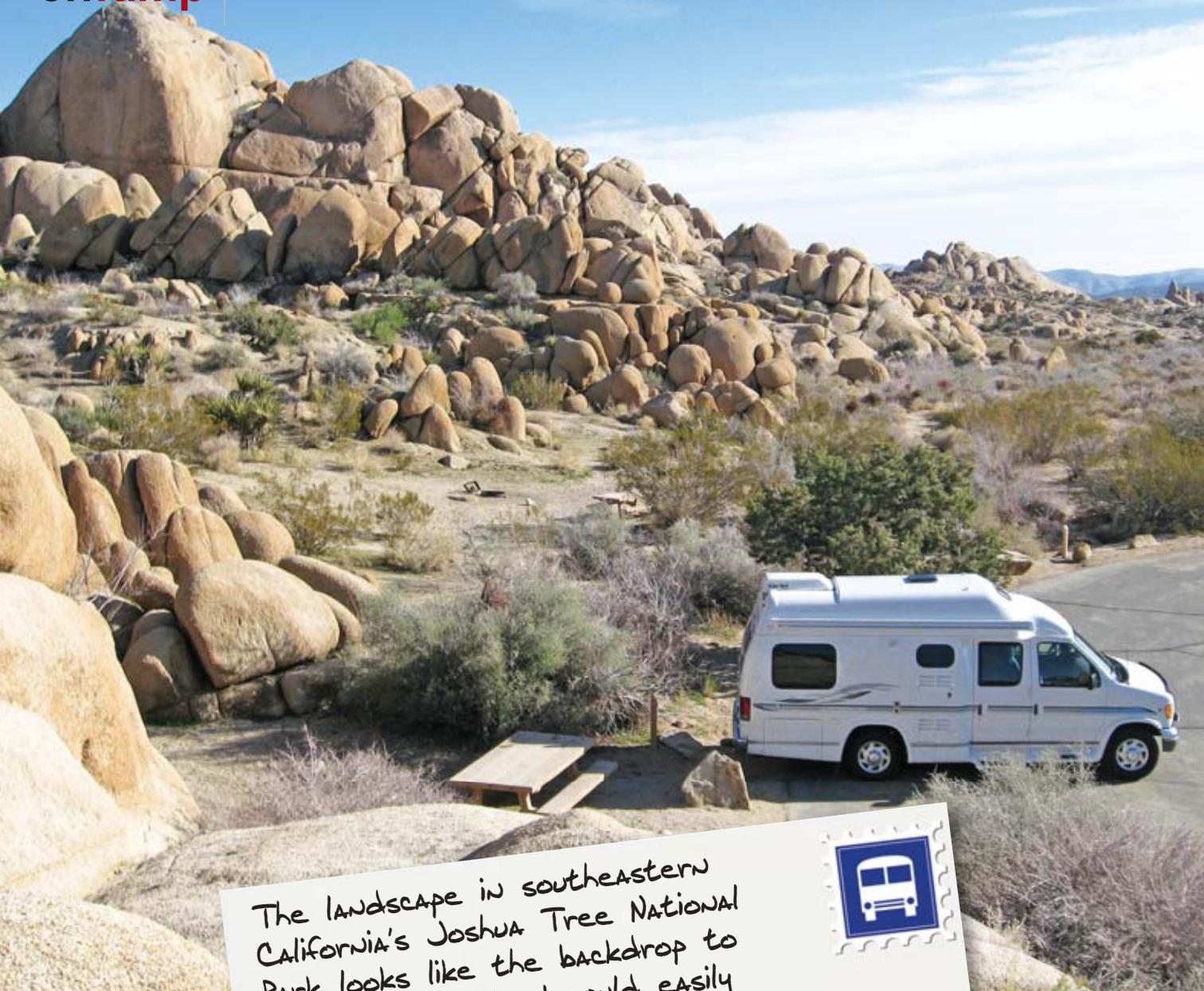
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The landscape in southeastern California's Joshua Tree National Park looks like the backdrop to a Western movie. I could easily imagine horses and cowboys running out from behind the rocks at any moment. It was great fun scrambling up the boulders, and the views got better the higher I climbed.

Patricia Bouta
Seeley Lake, Montana



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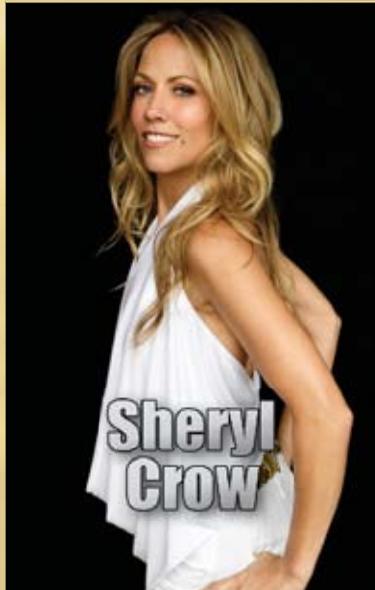
At the Good Sam Rally in Louisville, KY June 21-24, 2012



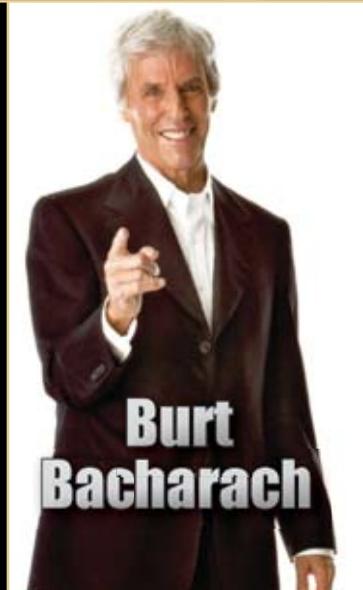
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